

**AGENDA**  
**Huron-Clinton Metropolitan Authority**  
**Board of Commission Meeting**  
**June 14, 2018 – 10:30 a.m.**  
**Lake St. Clair Metropark – Thomas Welsh Activity Center**

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1. Call to Order
2. Chairman's Statement
3. Public Participation
4. Approval – May 10, 2018 Regular Meeting and Closed Meeting Minutes
5. Approval – June 14, 2018 Full Agenda

**Consent Agenda**

6. **Approval – June 14, 2018 Consent Agenda**
  - a. Approval – May 2018 Financial Statements and Payment Registers
  - b. Approval – May 2018 Appropriation Adjustments **pg. 1**
  - c. Purchases
    1. Provision/Installation of Three (3) Truck Bodies, Kensington, Lake St. Clair, Stony Creek **pg. 3**
    2. Five (5) Utility Cart Vehicles, Lake Erie, Lower Huron, Willow, Oakwoods, Stony Creek **pg. 5**
    3. Chipper Truck, All Locations **pg. 7**
  - d. Report – Purchases Over \$10,000 **pg. 8**
  - e. Bids – Road Overlay and Repairs, Kensington **pg. 9**
  - f. Report – Marketing/Media Stats **pg. 14**

**Regular Agenda**

7. **Reports**
  - A. *Finance Department*
    1. Report – 2017 Audited Financial Statements **pg. 17**
  - B. *Purchasing Department*
    1. Report – Local Preference and Vendor Selection **pg. 18**
  - C. *Administrative Department*
    1. Approval – Motor City Canoe Service Agreement, Oakwoods **pg. 22**
  - D. *Planning Department*
    1. Approval – Willow and Oakwoods Master Plan **pg. 36**
    2. Bids – Tennis/Basketball Court Rehabilitation Contract, Various Locations **pg. 37**
    3. Approval – Sheldon Trails Redevelopment Design Contract, Stony Creek **pg. 40**
    4. Report – Lake St. Clair Beach Restoration **pg. 63**
  - E. *Natural Resources Department*
    1. Report – Metroparks Mowing Plan **pg. 64**
  - F. *Engineering Department*
    1. Bids – Maintenance Aboveground Storage Tank Installation, Huron Meadows **pg. 65**
    2. Approval – 26 Mile Road Bridge Rehabilitation/Pedestrian Bridge Design, Stony Creek **pg. 66**

**AGENDA**  
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**Board of Commission Meeting**  
**June 14, 2018 – 10:30 a.m.**  
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8. Election of Board Officers **pg. 143**
9. **Closed Session** – To consult with attorney(s) regarding pending litigation pursuant to MCL 15.268 Section 8 (e) of the Open Meetings Act.
10. Other Business
  - a. Class I and Class II eBike Demonstration **pg. 144**
11. Staff Leadership Update
  - a. Memorial Day Stats **pg. 145**
12. Commissioner Comments
13. Motion to Adjourn

The next regular Metroparks Board meeting will take place  
**Thursday, July 14, 2018 – 10:30 a.m.**  
Kensington Metropark – Nature Center



## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
From: Rebecca Franchock, Supervisor of Budget and Payroll  
Subject: Approval – May Appropriation Adjustments  
Date: June 7, 2018

### **Action Requested: Motion to Approve**

That the Board of Commissioners' approve the May 2018 Appropriation Adjustments as recommended by Supervisor of Budget and Payroll Rebecca Franchock and staff.

**Background:** The Metroparks ERP system provides a work-flow process to facilitate departmental budget management. Requested transfers are initiated by department staff and routed to the appropriate department head/district park superintendent for review and approval. Finance provides a final review of the approved requests to verify that they do not negatively impact Fund Balance.

For the month of May, \$37,134 represents funds transferred within and between the departments to cover over budget accounts or to move funds to the correct account. In addition, expenditure budgets increased by \$16,295 related to funds received from the Foundation or other donors; \$477 in donation funds were reflected in an increase to a revenue budget. Finally, there were various adjustments to taxes receivable resulting in a \$8,916 net increase.

The net impact on Fund Balance is a \$6,902 decrease, the result of which can be seen by the *accounting function and location* in the attached chart.

**Attachment: May Appropriation Adjustments**

## Huron-Clinton Metropolitan Authority May 2018 Appropriation Transfer Summary

### Expense Accounts

	<u>Location</u>	<u>Increase</u>	<u>Decrease</u>	<u>Difference</u>
<b>Capital</b>	Lake St. Clair	\$ 13,168	\$ 13,168	\$ -
	Kensington	-	5,210	(5,210)
	Lower Huron/Willow/Oakwoods	5,210	-	5,210
	Stony Creek	1,315	4,306	(2,991)
	<b>Total</b>	<b>\$ 19,693</b>	<b>\$ 22,684</b>	<b>\$ (2,991)</b>
<b>Major Maintenance</b>				
	Lower Huron	\$ 12,900	\$ 12,900	\$ -
	<b>Total</b>	<b>\$ 12,900</b>	<b>\$ 12,900</b>	<b>\$ -</b>
<b>Operations</b>				
	Kensington	\$ 50	\$ 50	\$ -
	Lower Huron	900	900	-
	Hudson Mills	600	600	-
	Stony Creek	2,991		2,991
	<b>Total</b>	<b>\$ 4,541</b>	<b>\$ 1,550</b>	<b>\$ 2,991</b>
<b>Total Expense</b>		<u><u>\$ 37,134</u></u>	<u><u>\$ 37,134</u></u>	<u><u>\$ -</u></u>

### May 2018 Foundation/Donation Transfer Summary

	<u>Expenditure Increase</u>	<u>Revenue Increase</u>	<u>Net</u>
<b>Operations</b>	Lake St. Clair	\$ -	\$ 2,000
	Kensington	-	700
	Lower Huron/Willow/Oakwoods	-	9,000
	Stony Creek	-	3,290
	Wolcott Mill	477	828
	<b>Total</b>	<b>\$ 477</b>	<b>\$ 15,818</b>

### May 2018 Net Tax Revenue Budget Adjustments

	<u>Expenditure Increase</u>	<u>Revenue Increase</u>	<u>Net</u>
<b>Tax Revenue</b>	Current	\$ 4,406	\$ (4,406)
	Prior	4,510	(4,510)
	<b>Total</b>	<b>\$ 8,916</b>	<b>\$ (8,916)</b>





# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Heidi Dziak, Senior Buyer  
 Project No: ITB 2018-025  
 Project Title: Purchase – Provision and Installation of Three (3) Truck Bodies  
 Location: Kensington, Lake St. Clair and Stony Creek Metroparks  
 Date: June 7, 2018

## Action Requested: Motion to Approve

That the Board of Commissioners' approve ITB 2018-025 to the low responsive, responsible bidders, as indicated below, for the purchase of truck work bodies for a total spend of \$80,987.40 as recommended by Senior Buyer Heidi Dziak and staff.

- Award of a purchase order in the amount of \$16,362 to Hoekstra Equipment of Grand Rapids, Michigan for the provision and installation of a Workport maintenance work body.
- Award of a purchase order in the amount of \$49,388 to Venchurs Vehicle Supply of Adrian, Michigan for the provision and installation of a Wayne Curbtender Quantum six-yard rear loader garbage collection work body.
- Award of a purchase order in the amount of \$15,237.40 to Southern Truck Equipment of Almont, Michigan for the provision and installation of a Voth fixed side dump work body and Western plow.

**Fiscal Impact:** Funds will come from the Board approved 2018 Capital Equipment budget, which provides a total of \$236,500 for the purchase of the three cab and chassis trucks and work bodies. The Board approved the purchase of the cab and chassis trucks for \$111,387. at the May meeting, leaving a balance of \$125,113 for the purchase of the work bodies. The total cost of the work bodies is \$80,987.40, under budget by \$44,125.60.

**Scope of Work:** Provide and install new truck work bodies to replace worn and aging equipment. The new truck work bodies will be installed on Metropark-owned chassis cabs.

<u>Description / Location / Unit Replaced</u>	<u>Work Body Cost</u>	<u>Available Budget</u>
<b><u>Kensington</u></b>		
1. New Maintenance Work Body for 2018 Ford F550 Regular Cab Chassis, 4x4	\$16,362.00	\$24,206.00
<ul style="list-style-type: none"> <li>• <i>Replaces #1: 2008 Ford F450 maintenance truck with 75,004 miles, rust and a shifting maintenance box which will be sold at auction.</i></li> </ul>		

*Entire budget for this vehicle: \$61,000.00*

## Provision and Installation of Three Truck Bodies

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<u>Description / Location / Unit Replaced</u>	<u>Work Body Cost</u>	<u>Available Budget</u>
<b><u>Lake St. Clair</u></b>		
2. New Garbage Collection Work Body for 2018 Ford F550 Regular Cab Chassis, 4x4	\$49,388.00	\$69,886.00
<ul style="list-style-type: none"> <li>Replaces #804: 2001 International garbage truck, with 16,623 miles, which will be transferred to Kensington.</li> </ul>		
<i>Entire budget for this vehicle: \$107,500.00</i>		
<b><u>Stony Creek</u></b>		
3. New Dump Work Body and Western Plow for 2018 Ford F550 Regular Cab Chassis, 4x4	\$15,237.40	\$31,021.00
<ul style="list-style-type: none"> <li>Replaces #355: 2000 GMC dump truck with 61,558 miles and rust, which will be sold at auction.</li> </ul>		
<i>Entire budget for this vehicle: \$68,000.00</i>		
<b>TOTAL</b>	<b>\$80,987.40</b>	<b>\$125,113.00</b>

**Background:** The proposed purchase of the trucks was competitively bid and posted on the Michigan Intergovernmental Trade Network (MITN) site, which provided notice of the solicitation to 55 vendors, from which 11 vendors downloaded the ITB.

<u>Vendor</u>	<u>Location</u>	<u>Price</u>
<b>Provide and Install Maintenance Work Body</b>		
<ul style="list-style-type: none"> <li>Hoekstra Equipment</li> <li>Venchurs Vehicle Supply</li> <li>NBC Truck Equipment</li> </ul>	Grand Rapids Adrian Roseville	\$16,362.00* \$16,727.00 \$18,179.00
<b>Provide and Install Garbage Collection Work Body</b>		
<ul style="list-style-type: none"> <li>Venchurs Vehicle Supply</li> </ul>	Adrian	\$49,388.00*
<b>Provide and Install Dump Work Body and Plow</b>		
<ul style="list-style-type: none"> <li>Southern Truck Equipment -offering Boss Plow</li> <li>Southern Truck Equipment</li> <li>Hoeskra Equipment</li> <li>Venchurs Vehicle Supply</li> </ul>	Almont  Almont Grand Rapids Adrian	\$14,817.00  \$15,237.40* \$16,942.00 \$19,629.00

(\*) indicated recommended award



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Heidi Dziak, Senior Buyer  
 Project No: ITB 2018-039  
 Project Title: Purchase – Five (5) Utility Cart Vehicles  
 Location: Lake Erie, Lower Huron/Willow/Oakwoods and Stony Creek Metroparks  
 Date: June 7, 2018

## Action Requested: Motion to Approve

That the Board of Commissioners approve ITB 2018-039 to Midwest Golf & Turf Acquisitions LLC of Commerce Township, Michigan, the low responsive, responsible bidder for the purchase of five (5) new utility carts in the total amount of \$47,270.04 as recommended by Senior Buyer Heidi Dziak and staff.

**Fiscal Impact:** Funds will come from the Board approved 2018 Capital Equipment budget, which allowed \$50,400 for the equipment purchases. The \$47,270.04 for the cost of the equipment is \$3,129.96 under budget.

**Scope of Work:** Furnish and deliver five (5) new pieces of equipment as follows:

<u>Description / Location / Unit Replaced</u>	<u>Cost</u>	<u>Budget</u>
<b><u>Lake Erie</u></b>		
1. Club Car Carryall 500 utility cart with range cab and ball picker attachment <ul style="list-style-type: none"> <li>Replaces #999, a 1987 Ford Tractor with 2584 hours which runs well but damages golf turf due to its weight. It will replace #1045, a 2006 Club Car Turf II with 3762 hours used by maintenance which will be sold at auction.</li> </ul>	\$11,227.54	\$12,000.00
2. Club Car Carryall 300 utility cart <ul style="list-style-type: none"> <li>Replaces #1289, a 1997 Club Car Carryall I with 1478 hours and motor problems which will be sold at auction.</li> </ul>	\$8,944.85	\$9,900.00
3. Club Car Carryall 500 utility cart <ul style="list-style-type: none"> <li>Replaces #1780, a 2001 Club Car Turf II with 3693 Hours, which will be sold at auction.</li> </ul>	\$8,991.45	\$9,500.00
<b><u>Lower Huron/Willow/Oakwoods</u></b>		
4. Club Car Carryall 300 utility cart (extra Maintenance Options) <ul style="list-style-type: none"> <li>Replaces #2014, a 2006 Club Car Turf I with 2493 hours, which will be sold at auction.</li> </ul>	\$9,161.35	\$9,500.00
<b><u>Stony Creek</u></b>		
5. Club Car Carryall 300 utility cart <ul style="list-style-type: none"> <li>Replaces #773, a 2007 Club Car Tuff II with 3010 hours, which will be sold at auction.</li> </ul>	\$8,944.85	\$9,500.00
<b>TOTAL</b>	<b>\$47,270.04</b>	<b>\$50,400.00</b>

## Purchase – Five (5) Utility Cart Vehicles

### Page 2

**Background:** The proposed purchase of the equipment was competitively bid and posted on the Michigan Intergovernmental Trade Network (MITN) site, which provided notice of the solicitation to 44 vendors, of which 11 vendors downloaded the ITB.

The Metroparks received bids for Cushman, Toro and Club Car equipment. After reviewing the product details of each equipment, staff is requesting approval to purchase Club Car equipment as it is built with aluminum and will better resist rust and damage from hot materials such as hot coals. In the case of the beverage cart, the Club Car has the size capacity to accommodate the Metroparks beverage insert whereas the other equipment does not.

<u>Item by Vendor</u>	<u>Location</u>	<u>Price</u>
<b>Club Car Carryall 500 with Range Cab and Ball Picker</b>		
• Spartan Distributors, Inc. ◦ offering Cushman 1200X model	Auburn Hills	\$ 9,895.00
• Spartan Distributors, Inc. ◦ offering Toro Workman 07042 model	Auburn Hills	\$10,485.40
• Midwest Golf & Turf Acquisitions LLC ◦ offering Club Car 500	Commerce Twp.	<b>\$11,227.54*</b>
<b>Club Car Carryall 300</b>		
• Spartan Distributors, Inc. ◦ offering Cushman 800X model	Auburn Hills	\$ 6,395.00
• Spartan Distributors, Inc. ◦ offering Toro Workman 07042 model	Auburn Hills	\$ 8,106.10
• Midwest Golf & Turf Acquisitions LLC ◦ offering Club Car 300	Commerce Twp.	<b>\$ 8,944.85*</b>
<b>Club Car Carryall 500 (replace Bev Cart)</b>		
• Spartan Distributors, Inc. ◦ offering Cushman 1200X model	Auburn Hills	\$ 7,045.00
• Spartan Distributors, Inc. ◦ offering Toro Workman 07042 model	Auburn Hills	\$ 7,765.17
• Midwest Golf & Turf Acquisitions LLC ◦ offering Club Car 500	Commerce Twp.	<b>\$ 8,991.45*</b>
<b>Club Car Carryall 300 (Extra Options)</b>		
• Spartan Distributors, Inc. ◦ offering Cushman 800X model	Auburn Hills	\$ 6,895.00
• Spartan Distributors, Inc. ◦ offering Toro Workman 07042 model	Auburn Hills	\$11,091.72
• Midwest Golf & Turf Acquisitions LLC ◦ offering Club Car 300	Commerce Twp.	<b>\$ 9,161.35*</b>

*(\*) indicates recommended award*



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Heidi Dziak, Senior Buyer  
 Project No: RFP 2018-023  
 Project Title: Amended Purchase – Chipper Truck  
 Location: All Locations  
 Date: June 7, 2018

## Action Requested: Motion to Amend Approval

That the Board of Commissioners amend award of RFP 2018-023 from Wolverine Freightliner – Westside of Ypsilanti, Michigan to the low responsive, responsible bidder, Tri-County International Trucks, Inc. of Dearborn, Michigan for the purchase of one new Chipper Truck in the amount of \$112,241 as recommended by Senior Buyer Heidi Dziak and staff.

**Fiscal Impact:** This purchase was included in the 2018 Board approved capital equipment budget in the amount of \$140,000. This purchase from Tri-County International Trucks is under budget and will have a positive net impact on the Fund Balance in the amount of \$27,759.

**Scope of Work:** Furnish and deliver one (1) chipper truck.

**Background:** The RFP was competitively bid and posted on the Michigan Intergovernmental Trade Network (MITN) site, which provided notice to vendors, of which 13 vendors downloaded the RFP. Three proposals were received in response to RFP 2018-023 and were found to match the specifications.

At the May 10 meeting, the Board approved RFP 2018-023 and awarded the purchase to Wolverine Freightliner. Upon receipt of the purchase order, the vendor determined it could not meet the lead time it proposed for the chipper truck. Tri-County International Trucks can meet the lead time and staff requests approval to purchase the chipper truck from Tri-County International Trucks.

The chipper truck replaces unit #34, a 2003 Sterling chipper truck that has 40,095 miles, electrical problems and a rusted body, which will be sold at auction.

<u>Vendor</u>	<u>Location</u>	<u>Price</u>
Wolverine Freightliner – Westside <i>Cannot meet delivery lead time</i>	Ypsilanti, MI	\$100,633.00
Tri-County International Trucks, Inc. <i>Bid #1 with upfitting from Truck &amp; Trailer Specialties</i>	Dearborn, MI	<b>\$112,241.00*</b>
<i>Bid #2 with upfitting from NBC Truck Equipment</i>		\$113,444.00

(\*) indicated recommended award



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Amy McMillan, Director  
 Project Title: Update – Purchases over \$10,000.00  
 Date: June 7, 2018

## Action Requested: Motion to Approve

That the Board of Commissioners receive and file the update for purchases over \$10,000, up to, and including \$25,000 as submitted by Director Amy McMillan and staff.

**Background:** On May 9, 2013, the Board approved the updated financial policy requiring the Director to notify the Board of purchases exceeding \$10,000, up to, and including \$25,000.

The following list is purchases exceeding the \$10,000 threshold:

<u>Vendor</u>	<u>Description</u>	<u>Price</u>
1. The Yellow Rose	Vinyl Siding Replacement on six (6) Comfort Stations Lower Huron Metropark	\$17,310.59
2. Paragon Design & Display	Light Post Banners and Hangers Various Locations	\$13,673.88
3. Weingartz Supply Company	Self-Contained Truck Loader (Leaf Vac) Lake St. Clair Metropark	\$13,564.00



## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Mike Henkel, Manager of Engineering  
 Project No: 704-18-353  
 Project Title: Bids – Road Overlay and Repairs  
 Project Type: Capital Improvement and Major Maintenance  
 Location: Kensington Metropark  
 Date: June 7, 2018

Bids Received: May 22, 2018

### **Action Requested: Motion to Approve**

That the Board of Commissioners' (1) award Contract No. 704-18-353 to the low responsive, responsible bidder, Ajax Paving Industries, Inc. in the amount of \$1,046,210.50; and (2) approve the transfer of \$46,210.50 from Fund Balance to cover the cost of the project as recommended by Manager of Engineering Mike Henkel and staff.

**Fiscal Impact:** This project is over budget by \$46,210.50.

**Scope of Work:** The work includes furnishing all labor, traffic control, equipment and materials necessary to repair and resurface approximately three miles of main park roadway with a two-inch overlay of asphalt.

**Background:** In 1995 (Route 3), the existing sections of roadway were paved, in 1997, (boat rental to playfield) and in 1988 (nature center to boat rental). At this time, staff recommends the repairs and resurfacing as part of ongoing roadway management program to preserve, prolong and maintain the integrity of the roadway.

In order to maintain the existing surface and prolong the life expectancy between reconstruction of the roadway profile subsequent overlays and repairs are necessary when the surface reaches the existing condition level. The condition level is assessed yearly and put into the Metroparks road asset management system. Based on the data, priorities are identified and projects are planned. To date, roadway overbanding has been used to seal pavement cracks due to weathering and the age of the binder of the wearing surface.

Staff is recommending providing a new wearing surface to preserve the original pavement structure and to extend the life of the roadway. The goal of the roadway management program is a cost saving effort by prolonging the surface as much as possible between more expensive roadway reconstructions.

## Bids – Road Overlay and Repairs

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<u>Contractor</u>	<u>City</u>	<u>Total</u>
Ajax Paving Industries, Inc.	Troy	\$1,046,210.50
Cadillac Asphalt, LLC	Wixom	\$1,071,833.00
James P. Contracting., Inc.	Washington	\$1,078,360.50
Asphalt Specialists, Inc.	Pontiac	\$1,088,006.00
Proline Asphalt Paving	Washington	\$1,145,639.00
Florence Cement Co.	Shelby Twp.	\$1,179,371.15

Budget Amount for Contract Services and Administration Road Reconstruction Parkwide	\$1,000,000.00
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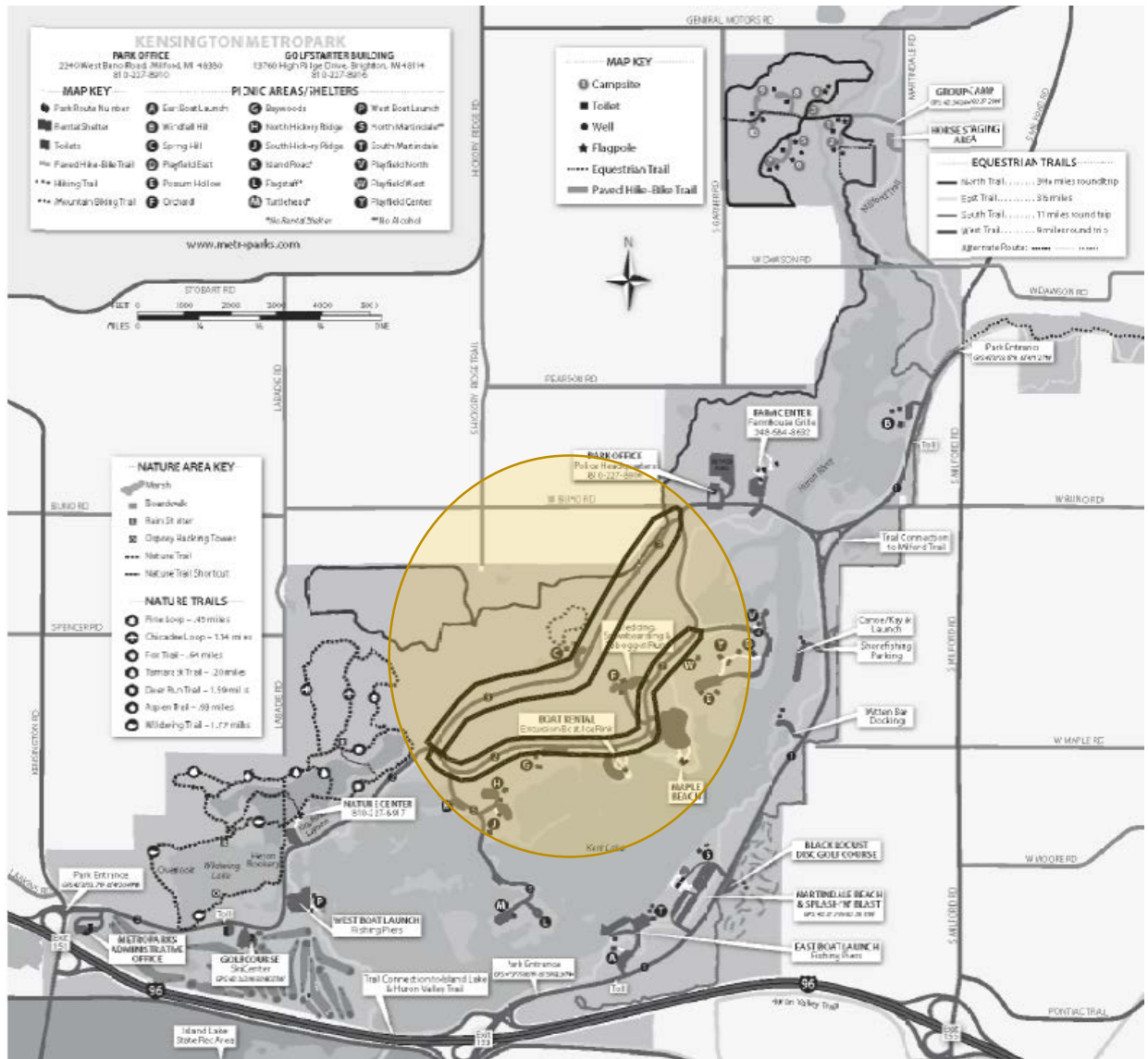
Work Order Amount	
Contract Amount-Ajax Paving Industries	\$1,046,210.50
Contract Administration	<u>\$ 10,000.00</u>
Total Proposed Work Order Amount (Rounded)	\$1,056,211.00

*This project was reported and publicly advertised in the following construction reporting outlets: MITN, Construction Association of Michigan, Reed Construction Data, Construction News Corporation, Construction News Service, Builders Exchange of Michigan, McGraw Hill Dodge, Builders Exchange of Lansing and Central Michigan, Washtenaw Contractors Association, Michigan ISQFT*

**Attachments:**      **Project Location**  
                             **Photos of Current Road Conditions**



## Project Location



## Current Road Conditions





## Current Road Conditions





## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
From: Hilary Simmet, Marketing and Media Specialist  
Subject: Report – Marketing/Media Stats  
Date: June 7, 2018

**Action Requested: Motion to Receive and File**

That the Board of Commissioners' receive and file the Marketing and Media statistics report as recommended by Marketing and Media Specialist Hilary Simmet and staff.

**Background:** An overview of marketing and media efforts through April are attached for review.

**Attachment: Marketing and Media Stats**

## 2018 Marketing/Media Stats

### PAID MEDIA

	Print Ads	Digital Ads
January	5	3
February	5	3
March	7	4
April	8	6

### UNPAID MEDIA (ORGANIC)

	Total Online Mentions	Total Online Visitors	Total Online Value	Total TV Mentions	Total TV Visitors	Total TV Value
January	42	3,069,749	\$ 52,014			
February	69	3,931,997	\$ 41,378			
March	59	1,852,569	\$ 15,897			
April	136	57,158,304	\$1,719,057	10	73,132	\$ 3,883

### SOCIAL MEDIA - MAIN FACEBOOK

	Followers	Number of Posts	Total Post Reach	Total Impressions	Total Reactions, Comments, Shares	Total Post Link Clicks
January	8,563	19	59,277	97,572	840	186
February	8,610	24	69,948	106,430	944	268
March	8,696	25	91,953	131,049	1,704	198
April	8,888	22	76,442	106,259	1,617	230

### SOCIAL MEDIA - TWITTER

	Followers	Number of Posts	Total Post Impressions	Total Post Engagement
January	1,821	28	16,346	598
February	1,861	25	14,509	647
March	1,895	35	21,362	854
April	1,926	39	27,445	1,179

### SOCIAL MEDIA - INSTAGRAM

	Followers	Number of Posts	Total Post Likes	Total Post Comments
January	574	10	414	9
February	620	12	486	10
March	971	14	522	6
April	729	11	395	1

## 2018 Marketing/Media Stats

### WEBSITE

	Page Views	Online Sales	Organic Search	Direct Search	Social Media Referral	Email
January	107,113	\$ 15,294	18,867	7,263	3,590	1,654
February	112,779	\$ 11,831	20,509	7,430	3,120	2,354
March	157,640	\$ 16,105	27,571	9,808	2,753	3,542
April	185,445	\$ 16,502	35,945	10,223	2,474	2,862

### COMMUNITY OUTREACH

	Presentations	Cold Calls	Networking Meetings	One on One Meetings
January	4	83	8	2
February	5	95	10	2
March	11	82	5	5
April	4	65	6	3



## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Rebecca Franchock, Supervisor of Budget and Payroll  
 Subject: Report – 2017 Audited Financial Statements  
 Date: May 24, 2018

### **Action Requested: Motion to Receive and File**

That the Board of Commissioners' receive and file the 2017 Audited Financial Statements Report as recommended by Franchock and staff.

**Summary:** The Authority's auditing firm, Yeo and Yeo, has completed their audit of the Metroparks 2017 accounting records, pension plan, retiree health care trust and related financial statements. Together with Yeo and Yeo's Audited Financial Statements, staff compiled the Authority's 2017 Comprehensive Annual Financial Report, which is enclosed for review. Mr. Alan Panter will review the 2017 Audited Financial Statements with the Board at the June 7, 2018 meeting.

**Background:** Staff is pleased to report that Yeo and Yeo have issued an unqualified opinion, meaning the Audited Financial Statements present fairly the financial position of the governmental activities and each major fund of the Authority as of Dec. 31, 2017 in conformity with generally accepted accounting principles. Yeo and Yeo conducted their audit following generally accepted auditing standards in order for them to obtain reasonable assurance that the Authority's financial statements are free of any material misstatements.

As reflected on the Government Wide Statement of Net Position, the Authority's total net position at \$224.2 million an increase of \$0.4 million from the 2016 net position of \$223.8 million. The total General Fund Balance is reported at \$37.9 million, a decrease of \$3.0 million. The decrease to Fund Balance is a result of use of Fund Balance for Capital and Major Maintenance projects and increase in revenue over expenditures.

The 2017 Committed Fund Balance totals \$6.1 million. This represents Committed for Land Acquisition (\$5.4 million) and Committed for Rate Stabilization (\$0.7 million). The 2017 Assigned Fund Balance totals \$12.1. This represents Assigned for Compensated Absences (\$3.8 million), Assigned for Encumbrances (\$1.1 million) and Assigned for Planned Use of Fund Balance (\$7.2 million). The 2017 Unassigned Fund Balance stands at \$18.3 million. This level of Unassigned Fund Balance represents 33 percent of General Fund expenditures. The Statement of Revenues and Expenditures shows 2017 General Fund expenditures at \$55 million against revenues of \$51.9 million producing a deficit of \$3.1 million.

The Authority intends to submit this 2017 Comprehensive Annual Financial Report (CAFR) to the Government Finance Officers Association Certificate of Excellence in Financial Reporting program. The Authority has received a Certificate of Achievement for the last 16 years and staff believes this 2017 report will continue to earn this distinction. This achievement would not be possible without the dedicated work efforts of the entire Finance Department.

### **Separate Attachment: 2017 Audited Financial Statements**



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Heidi Dziak, Senior Buyer  
 Project Title: Report – Local Preference and Vendor Selection  
 Date: June 7, 2018

## Action Requested: Motion to Receive and File

That the Board of Commissioners' receive the Local Preference and Vendor Selection report as recommended by Senior Buyer Heidi Dziak and staff.

**Background:** Staff has prepared this report on local preference and vendor selection per the Board's request for more information about the practices and policies of county and city government offices and public agencies in Livingston, Macomb, Oakland, Washtenaw and Wayne counties.

The Metroparks does not have a local preference policy. Information contained in this report comes from the following organizations, arranged by county:

Livingston County:	Livingston County Government City of Brighton
Macomb County:	Macomb County Government City of Sterling Heights
Oakland County:	Oakland County Government City of Farmington Hills City of Troy
Washtenaw County:	Washtenaw County Government City of Ann Arbor
Wayne County:	Wayne County Government City of Dearborn City of Detroit City of Livonia Great Lakes Water



## 1. General Types of Local Preference Policy Identified

The referenced entities represent a wide range of policies related to local preference and vendor selection and the policies are categorized below:

- No Policy

Of the above referenced entities, three have no policy expressing preference for vendors based on vendor's location or geography:

- City of Brighton (Livingston)
- City of Farmington Hills (Oakland)
- City of Sterling Heights (Macomb)
- Great Lakes Water (Wayne)

- Local Vendor If All Else is Equal (Tie Bids)

While no official local-preference policy, when aspects of bids and/or proposals are equal or nearly equal, preference may be given to a local vendor.

- City of Ann Arbor (Washtenaw)
- Livingston County Government

- Local Vendor When Quick Response or Emergency Exists

The location of the vendor is used as selection criteria when emergency repairs are required or the vendor must be able to provide a quick response.

- Oakland County Government

- Local Vendor Within a Percent **May** Meet Low Bid

If the local vendor is within a certain percent of the lowest bid offered by a non-local vendor, the local vendor is offered a chance to match the lowest bid and will then be awarded the bid.

For the entities that have this policy, the policy applies only to Bids and the local vendor's bid must be within 5 percent of the lowest bid.

- City of Dearborn (Wayne)
  - This policy has been in place for at least seven years. To date, it has not been used once.
- City of Troy (Oakland)

- Local Vendor if Within a Percent of Low Bid

If the local vendor is within a certain percent of the lowest bid offered by a non-local vendor, the local vendor may be awarded.

- City of Livonia (Wayne)
  - In contracts up to \$100,000, if the local vendor is within 3 percent of the lowest bid offered by a non-local vendor.
  - In contracts over \$100,000, if the local vendor is within 1 percent of the lowest bid offered by a non-local vendor.
  - Offers local vendors credit based on the contract: a 5 percent credit for contracts up to \$10,000; a 4 percent credit for contracts over \$10,000 and up to \$100,000; a 3 percent credit for contracts over \$100,000 and up to \$500,000; and a 2 percent credit for contracts over \$500,000.

- Local Vendor Credit

Some entities provide additional credit for local vendors when evaluating bids and proposals.

- City of Detroit (Wayne)
  - Offers local vendors credit based on the contract: a 5 percent credit for contracts up to \$10,000; a 4 percent credit for contracts over \$10,000 and up to \$100,000; a 3 percent credit for contracts over \$100,000 and up to \$500,000; and a 2 percent credit for contracts over \$500,000.
  - If the vendor is both a Detroit-based business and a Detroit-resident business, the above factors may be doubled.
- Macomb County Government
  - Offers local vendors credit based on the contract: a 5 percent credit for contracts up to \$50,000; a 3 percent credit for contracts over \$50,000 and up to \$200,000; and a 1 percent credit for contracts over \$200,000.
- Washtenaw County Government
  - Offers local vendors credit based on the contract. The percent of the credit relative to a contract amount changes per the solicitation.
- Wayne County Government
  - Offers the County Based Enterprises (CBE) program through which local businesses may apply and, if accepted, qualify for extra credit on the bids and proposals they submit to the county.

## **2. Identifying Local Vendors**

Entities define what makes a vendor “local” in different ways. Factors may include, but are not limited to:

1. Location of the business headquarters.
2. If the vendor has paid property tax on real or personal property in the jurisdiction.
3. What percent of the vendor’s regular full-time employees are based at the county location to perform on the proposed contract.
4. The extent to which a local vendor would sublet work to any non-local sub-contractor.

## **3. Federal Funds and Local Preference**

Federal funds may come with stipulations that local or geographical preference may not be used to award contracts these funds support. These restrictions apply to local funds that may be used in conjunction with federal funds. For instance, the Federal Transit Administration (FTA) does not allow any of its funds, regardless of the amount, to be used in purchases and contracts where geographical preference was considered.

## **4. HCMA Awards by Local and Non-Local Vendors**

Of 33 solicitations (Invitations to Bid, Requests for Proposals) the Metroparks has awarded this year, 58 percent of the bids and proposals received were offered by local vendors (vendors within the five counties); 42 percent were offered by non-local vendors (vendors outside the five counties).

Of the same solicitations, 61 percent of the dollars awarded went to local vendors; 39 percent went to non-local vendors.

HCMA has not maintained records based on vendor location, but has revisited bid and award information to provide data for this report and will continue to measure the same moving forward.

## **5. Advantages and Disadvantages of Local Preference Policies**

In 2015, NIGP: The Institute for Public Procurement published a position paper titled “Local Preference in Public Procurement – The Importance of Best Value Analysis When Government Has Adopted Local Procurement Preferences.” In the paper, the organization states:

*“NIGP: The Institute for Public Procurement maintains the position that preference policies, including local preferences, conflict with the fundamental public procurement principles of impartiality and full and open competition. Therefore, NIGP does not support the use of preference policies.”*

The document also included the following advantages and disadvantages of local preference programs:

### Advantages

1. Achieving local social policy goals to assist the local economy
2. Improving and protecting the local economy

### Disadvantages

1. Increased cost to the local taxpayers and government to implement such a program
2. Limiting supplier competition
3. Reducing the incentive for local vendors to provide the best value
4. Affecting, complicating and potentially burdening the procurement administrative processes
5. Defining a defensible fair process to determine the definition of a local business

## **6. Summary**

Six of the 14 entities referenced in this report have no local preference policy or award to a local vendor only in the event of a tie bid where all factors are equal.

One entity allows local vendor preference in emergencies or when a quick response is necessary.

The remaining seven entities have some mechanism to provide local vendors a greater opportunity to earn business, whether by allowing local vendors within some range of the low bid the opportunity to meet the low bid, or by giving greater evaluation credit to local vendors to strengthen their bids and/or proposals.

Further discussion with the entities referenced may be helpful should the Metroparks wish to further understand the implications of a local preference policy.

**HURON-CLINTON METROPOLITAN AUTHORITY**

To: Board of Commissioners  
From: Dave Kirbach, Deputy Director  
Subject: Approval - Motor City Canoe Rental Services Agreement  
Location: Oakwoods Metropark  
Date: June 7, 2018

**Action Requested: Motion to Approve**

That the Board of Commissioners' approve the service agreement with Motor City Canoe Rentals LLC as recommended by Deputy Director Dave Kirbach and staff.

**Fiscal Impact:** No negative fiscal impact with the potential for positive impact on revenue.

**Background:** Motor City Canoe Rentals LLC approached staff regarding renting canoes at Oakwoods Metropark in the Cedar Knoll Picnic area. The agreement is for a one-year trial basis in order for staff to evaluate the vendor and to see if there is a sufficient need for this service.

**Attachment: Motor City Canoe Rental Service Agreement**

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HURON-CLINTON METROPOLITAN AUTHORITY

## Concessionaire Contract

CONTRACT EXPIRATION DATE: December 31, 2018

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This "Contract" made this \_\_\_\_\_ day of \_\_\_\_\_, 2018 between the Huron-Clinton Metropolitan, a Michigan public body corporate, whose address is 13000 High Ridge Drive, Brighton, Michigan 48114, and the "Contractor" as further described in the following Table. In this Contract, either Contractor or HCMA (as defined herein) may also be referred to individually as "Party" or collectively, as "Parties".

HURON-CLINTON METROPOLITAN AUTHORITY 13000 High Ridge Drive Brighton, MI 48114 Tel: (810) 227-2757 Fax: (810) 227-7512 (herein, "HCMA")	MOTOR CITY CANOE RENTALS, L.L.C 24500 Goddard Road Taylor, MI 48180 Tel: (313) 473-9847 Identification No. <u>E5787W</u> (herein the "Contractor")
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INTRODUCTION

A. The HCMA owns and operates the following facilities:

- Lower Huron Metropark, 17845 Savage Rd.; Belleville, MI (mailing address)
- Willow Metropark, 17845 Savage Rd.; Belleville, MI 48111 (mailing address)
- Oakwoods Metropark, 17845 Savage Rd.; Belleville, MI (mailing address)

B. Subject to the terms and conditions set forth herein, HCMA desires to grant a license to utilize space (as designated by HCMA) at Lower Huron, Willow and Oakwoods Metroparks to the Contractor and to grant to the Contractor the right to provide certain services at said locations under the terms and conditions set forth in this Contract.

This Contract is organized and divided into the following "Section" or "Sections" for the convenience of the Parties.

SECTION 1. CONTRACT DOCUMENTS AND DEFINITIONS

SECTION 2. CONTRACT EFFECTIVE DATE AND TERMINATION

SECTION 3. SCOPE OF CONTRACTOR'S SERVICES

SECTION 4. USE OF HCMA FACILITIES AND PROPERTY

SECTION 5. PAYMENT OBLIGATIONS

SECTION 6. CONTRACTOR'S ASSURANCES AND WARRANTIES

SECTION 7. CONTRACTOR PROVIDED INSURANCE AND INDEMNIFICATION

SECTION 8. GENERAL TERMS AND CONDITIONS

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In consideration of the mutual promises, obligations, representations, and assurances in this Contract, the Parties agree to the following:

## **1. CONTRACT DOCUMENTS AND DEFINITIONS**

The following words and expressions when printed with the first letter capitalized as shown herein, whether used in the singular or plural, possessive or non-possessive, and/or either within or without quotation marks, shall be defined and interpreted as follows:

**1.1.** "Services" shall mean the following items:

**1.1.1.** Rental items which will include but not be limited to:

- Canoes
- Kayaks
- Tubes

**1.1.2.** Porter service between Lower Huron, Willow and Oakwoods for individuals with their own equipment

**1.2.** "Contractor Employee" means without limitation, any employees, officers, directors, members, managers, trustees, volunteers, attorneys, and representatives of Contractor, and also includes any Contractor licensees, concessionaires, contractors, subcontractors, independent contractors, contractor's suppliers, subsidiaries, joint ventures or partners, and/or any such persons, successors or predecessors, employees, (whether such persons act or acted in their personal, representative or official capacities), and/or any and all persons acting by, through, under, or in concert with any of the above. "Contractor Employee" shall also include any person who was a Contractor Employee at any time during the term of this Contract but, for any reason, is no longer employed, appointed, or elected in that capacity.

**1.3.** "Claims" means any alleged losses, claims, complaints, demands for relief or damages, suits, causes of action, proceedings, judgments, deficiencies, liability, penalties, litigation, costs, and expenses, including, but not limited to, reimbursement for reasonable attorney fees, witness fees, court costs, investigation expenses, litigation expenses, amounts paid in settlement, and/or other amounts or liabilities of any kind which arise or related to the Services provided by Contractor and are imposed on, incurred by, or asserted against the HCMA, or for which the HCMA may become legally and/or contractually obligated to pay or defend.

**1.4.** "Contract Documents" This Contract includes and fully incorporates herein all of the following documents:

**1.4.1.** Exhibit I: Contractor Insurance Requirements.

**1.5.** "HCMA" means the Huron-Clinton Metropolitan Authority, a Michigan public body corporate, its departments, divisions, authorities, boards, committees, and "HCMA Agent" as defined below.

**1.6.** "HCMA Agent" means all appointed officials, directors, board members, commissioners, employees, volunteers, representatives, and/or any such persons' successors (whether such person act or acted in their personal representative or official capacities), and/or any persons acting by, through, under, or in concert with any of them. "HCMA Agent" shall also include any person who was a "HCMA Agent" anytime during the term of this Contract but, for any reason, is no longer employed, appointed, or elected and serving as an Agent.

- 1.7. "Day" means any calendar day, which shall begin at 12:00:01 a.m. and end at 11:59:59 p.m.
- 1.8. "Facility" means a building, or property, as designated by HCMA located at Lower Huron, Willow and/or Oakwoods Metropark.
- 1.9. "HCMA Board of Commissioners" means the HCMA commission established in accordance with Michigan Public Act 147 of the Michigan Public Acts of 1939, as amended, Michigan Compiled Laws 119.51 et seq.

## **2. CONTRACT EFFECTIVE DATE AND TERMINATION**

- 2.1. The effective date of this Contract shall be\_\_\_\_\_, and unless otherwise terminated or canceled as provided herein, it shall end at 11:59:59 p.m. on the "Contract Expiration Date" shown on the first page of this Contract, at which time this Contract expires without any further act or notice of either Party being required. The Parties are under no obligation to renew or extend this Contract after Contract Expiration Date. At the expiration of the Contract unless otherwise extended, the Contract shall be bid through HCMA, according to its policies and procedures. Notwithstanding the above, under no circumstances shall this Contract be effective until and unless:
  - 2.1.1. This Contract is signed by a Contractor Employee, legally authorized to bind the Contractor.
  - 2.1.2. Any and all Contractor Certificates of Insurance and any other conditions precedent to the Contract have been submitted and accepted by the HCMA.
  - 2.1.3. This Contract is signed by an authorized agent of the HCMA, as provided for on the signature page of this Contract, who shall be the final signatory to this Contract.
- 2.2. Termination. HCMA may terminate and/or cancel this Contract (or any part thereof) at any time during the term, any renewal, or any extension of this Contract, upon thirty (30) days written notice to the Contractor, for any reason, including convenience without incurring obligation or penalty of any kind. The effective date for termination or cancellation shall be clearly stated in the written notice.
- 2.3. Contractor may terminate and/or cancel this Contract (or any part thereof) at anytime upon sixty (60) days written notice to HCMA, if HCMA defaults in any obligation contained herein, and within the sixty (60) notice period the HCMA has failed or has not attempted to cure any such default. The effective date of termination and/or cancellation and the specific alleged default shall be clearly stated in the written notice
- 2.4. In the event of termination and/or cancellation by HCMA for any reason, Contractor shall pay the HCMA all fees as set forth herein until the effective date of termination. Upon termination, cancellation and/or expiration of this Contract, Contractor's use of HCMA Property shall cease as of the effective date of termination cancellation and/or expiration.
- 2.5. Under no circumstances shall the HCMA be liable for any future loss of income, profits, any consequential damages or any loss of business opportunities, revenues, or any other economic benefit Contractor may have realized but for the termination and/or cancellation of this Contract. The HCMA shall not be obligated to pay Contractor any cancellation or termination fee if this Contract is cancelled or terminated as provided herein.

### **3. SCOPE OF CONTRACTOR'S SERVICES**

- 3.1.** Contractor shall perform the Services as defined herein at the Facility, not less than described in section 3.4.
- 3.2.** Equipment and Supplies. Contractor is responsible for providing all equipment and supplies to deliver the Services required by this Contract, which are not expressly required to be provided by the HCMA herein.
- 3.3.** Personal Floatation Devices and Waiver. Approved PFD's are required for all rentals. Contractor shall require every customer/renter to execute an HCMA-provided waiver of liability and indemnification agreement for inherently dangerous activities (a "Waiver"). Contractor shall maintain all original Waivers for the term of this Contract, shall make all or any of the Waivers available to HCMA upon request, and shall transfer all Waivers to HCMA's custody on or before the Contract Expiration Date or the effective date of any earlier termination.
- 3.4.** Days/Hours of Operation. Minimum days/hours of on-site operation will be:
  - 3.4.1.** May: Weekends and Holidays, 10am – 5pm or by appointment
  - 3.4.2.** June, July, August: Seven days per week, 9am – 6pm
  - 3.4.3.** September and October: Weekends and Holidays, 10am – 5pm or by appointment

### **4. USE OF HCMA FACILITIES AND PROPERTY**

- 4.1.** Contractor may use and have access to the Facility described and depicted in this contract to provide the Services.
- 4.2.** The HCMA may access the Facility at any time for the purpose of examining and inspecting the Facility and evaluating the Services provided pursuant to this Contract. If the HCMA determines that the Facility is not maintained pursuant to this Contract or Services are not provided pursuant to this Contract, it shall immediately notify Contractor in writing to correct the unsatisfactory conditions or Services. Contractor shall take immediate steps to correct such conditions or Services.
- 4.3.** Contractor agrees not to advertise its Services with HCMA in any manner or form, on or at the Facility, HCMA premises, or other location; or in any newspapers, website or through the use of electronic media, without the prior written consent of the HCMA Director or his or her designee. Contractor shall not employ or use any persons known as "hawkers", "spielers", "crier" or other noise makers or means of attracting attention to Contractor's business, unless approved in writing by HCMA Director or his or her designee.
- 4.4.** Contractor shall keep the Facility and anything stored thereon in good order and repair and in a clean, safe and healthful condition as required by this Contract and as required by federal, state or local, law, rule, regulation or ordinance.
- 4.5.** Except as otherwise provided in this Contract and unless prior written approval is given by the HCMA Director or his or her designee, Contractor shall not make any alterations, additions, or changes to the Facility.
- 4.6.** At the expiration or termination of this Contract, Contractor shall leave the Facility in the same condition that Contractor found them and clean of all rubbish. Contractor shall remove all of its personal property within thirty (30) days of expiration or termination of this Contract. If Contractor does not remove its personal property within the thirty (30)



day period, the HCMA shall dispose of it as it sees fit and Contractor shall reimburse the HCMA for all reasonable costs associated with the disposal of the personal property upon receipt of an invoice from HCMA.

- 4.7. Damage to HCMA Facilities. Contractor shall be responsible for any damage to the Facilities or other HCMA property that is caused by the negligence of Contractor or Contractor Employees. If damage occurs, Contractor shall notify the HCMA immediately and the HCMA shall make the necessary repairs and/or replacements or cause a third party to make the necessary repairs or replacements, provided, however, that upon receipt of an invoice from the HCMA, Contractor shall reimburse the HCMA for all reasonable costs associated with repairing and/or replacing the Facilities or other HCMA owned property.
- 4.8. Damage to Contractor Property. Contractor shall be solely liable and responsible for any property loss or damage resulting from fire, theft or other means to Contractor's personal property located, kept, or stored on or around the Facilities during this Contract.
- 4.9. Contractor shall be solely liable and responsible for any Claims, occurring at or around the Facilities, which arise out of Contractor's or Contractor's Employees use of the Facilities or performance of Services under this Contract.
- 4.10. Contractor acknowledges that it has no title in or to the Facility or any portion thereof and will not claim any such title to the Facility.

## 5. PAYMENT OBLIGATIONS

- 5.1. Contractor shall pay HCMA 20% of gross revenue, payable in monthly installments in exchange for the license to provide Services as set forth in this Contract. Days/Months of operation will not be less than defined in section 3.4..
- 5.2. The Monthly Fee shall be due and payable on the 15<sup>th</sup> of each calendar month while in operation.
- 5.3. Late Charge. If the HCMA does not receive the Monthly Fee or any other sum owed by Contractor under this Contract within five (5) days after its due date, Contractor shall pay the HCMA a late charge equal to five percent (5%) of any such overdue amount. Such late charge represents a fair and reasonable estimate of the costs the HCMA will incur by reason of late payment by the HCMA. Acceptance of such late charge by the HCMA shall in no event constitute a waiver of Contractor's default with respect to such overdue amount, nor prevent the HCMA from exercising any of its other rights and remedies.
- 5.4. Under no circumstances shall the HCMA be responsible for any cost, fee, fine, penalty, or direct, indirect, special, incidental or consequential damages incurred or suffered by Contractor in connection with or resulting from Contractor's provision of Services under this Contract.
- 5.5. The HCMA has the right to offset any amounts due and owing to the Contractor should the HCMA incur any cost associated with this Contract that is the obligation of Contractor under this Contract.

## 6. CONTRACTOR'S ASSURANCES AND WARRANTIES

- 6.1. Service Warranty. Contractor warrants that all Services performed hereunder will be performed in a manner that complies with all applicable laws, statutes, regulations, ordinances, and professional standards.

- 6.2. Taxes.** The Contractor shall pay, its own local, state and federal taxes, including without limitation, taxes by reason of this Contract, social security taxes, and unemployment compensation taxes. The HCMA shall not be liable to or required to reimburse the Contractor for any federal, state and local taxes or fees of any kind.
- 6.3. Contractor's Incidental Expenses.** Except as otherwise expressly provided in this Contract, the Contractor shall be solely responsible and liable for all of Contractor's costs and expenses incident to the performance of all Services for the HCMA including, but not limited to, any professional dues, association fees, license fees, fines, taxes, and penalties.
- 6.4. Contractor Employees.**
- 6.4.1.** Contractor shall employ and assign qualified Contractor Employees as necessary and appropriate to provide the Services under this Contract. Contractor shall ensure all Contractor Employees have all the necessary knowledge, skill, and qualifications necessary to perform the required Services and possess any necessary licenses, permits, certificates, and governmental authorizations as may be required by law.
- 6.4.2.** Contractor shall solely control, direct, and supervise all Contractor Employees with respect to all Contractor obligations under this Contract. Contractor will be solely responsible for and fully liable for the conduct and supervision of any Contractor Employee.
- 6.4.3.** All Contractor Employees assigned to work under this Contract may, at the HCMA's discretion, be subject to a security check and clearance by the HCMA.
- 6.5. Contractor Employee-Related Expenses.** All Contractor Employees shall be employed at the Contractor's sole expense (including employment-related taxes and insurance) and the Contractor warrants that all Contractor Employees shall fully comply with and adhere to all of the terms of this Contract. Contractor shall be solely and completely liable for any and all applicable Contractor Employee's federal, state, or local payment withholdings or contributions and/or any and all Contractor Employee related pension or welfare benefits plan contribution under federal or state law. Contractor shall indemnify and hold the HCMA harmless for all Claims against the HCMA by any Contractor Employee, arising out of any contract for hire or employer-employee relationship between the Contractor and any Contractor Employee, including, but not limited to, Worker's Compensation, disability pay or other insurance of any kind.
- 6.6. Full Knowledge of Service Expectations and Attendant Circumstances.** The Contractor is responsible for being adequately and properly prepared to execute this Contract. Contractor has satisfied itself in all material respects that it will be able to perform all obligations under the Contract as specified herein.
- 6.7. The Contractor's Relationship To The HCMA Is That Of An Independent Contractor.** Nothing in this Contract is intended to establish an employer-employee relationship between the HCMA and either the Contractor or any Contractor Employee. All Contractor Employees assigned to provide Services under this Contract by the Contractor shall, in all cases, be deemed employees of the Contractor and not employees, agents or sub-contractors of the HCMA.

## **7. CONTRACTOR PROVIDED INSURANCE AND INDEMNIFICATION**

### **7.1. Indemnification.**

- 7.1.1.** Contractor shall indemnify, defend and hold HCMA harmless from any and all Claims which are incurred by or asserted against HCMA by any person or entity alleged to have been caused or found to arise, from the acts, performances, errors,

or omissions of Contractor or Contractor's Employees, including, without limitation, all Claims relating to injury or death of any person or damage to any property.

**7.1.2.** The indemnification rights contained in this Contract are in excess and over and above any valid and collectible insurance rights/policies. Contractor and HCMA shall have no rights against each other for any indemnification (e.g., contractual, equitable, or by implication), contribution, subrogation, and/or any other right to be reimbursed except as expressly provided herein.

**7.1.3.** Contractor waives and releases all actions, liabilities, loss and damage including any subrogated rights it may have against the HCMA based upon any Claim brought against the HCMA suffered by a Contractor Employee.

**7.2.** Contractor Provided Insurance. At all times during this Contract, Contractor shall obtain and maintain insurance according to the specifications indicated in Exhibit I.

## **8. GENERAL TERMS AND CONDITIONS**

**8.1.** Cumulative Remedies. A Party's exercise of any remedy shall not preclude the exercise of any other remedies, all of which shall be cumulative. A Party shall have the right, in its sole discretion, to determine which remedies are to be exercised and in which order.

**8.2.** Survival of Terms and Conditions. The following terms and conditions shall survive and continue in full force beyond the termination and/or cancellation of this Contract (or any part thereof) until the terms and conditions are fully satisfied or expire by their very nature:

"CONTRACTOR'S ASSURANCES AND WARRANTIES";

"CONTRACTOR PROVIDED INSURANCE AND INDEMNIFICATION";

"Damage Clean Up To HCMA Property and/or Premises";

"Severability";

"Governing Law/Consent To Jurisdiction And Venue"; and

"Survival of Terms And Conditions".

**8.3.** HCMA Right to Suspend Services. Upon written notice, the HCMA may suspend performance of this Contract if Contractor has materially failed to comply with Federal, State, or Local laws, or any requirements contained in this Contract. The right to suspend services is in addition to the HCMA's right to terminate and/or cancel this Contract. The HCMA shall incur no penalty, expense, or liability to Contractor if the HCMA suspends services under this Section.

**8.4.** No Third Party Beneficiaries. Except as provided for the benefit of the Parties, this Contract does not and is not intended to create any obligation, duty, promise, contractual right or benefit, right to be indemnified, right to be subrogated to the Parties' rights in this Contract, and/or any other right, in favor of any other person or entity.

**8.5.** Compliance with Laws. Contractor shall comply with all federal, state, and local laws, statutes, ordinances, regulations, rules, insurance policy requirements, and requirements applicable to its activities under this Contract, including but not limited to the Michigan Liquor Control Code, as amended, and all regulations and rules promulgated thereunder.

**8.6.** Permits and Licenses. Contractor shall be responsible for obtaining, maintaining, and paying for all licenses, permits, certificates, and governmental authorizations necessary to perform its obligations under this Contract and to conduct business under this Contract.

- 8.7. Discrimination.** Contractor shall not discriminate against any employee or applicant for employment because of sex, race, religion, color, national origin, or handicap in violation of State and Federal law.
- 8.7.1.** Contractor shall promptly notify the HCMA of any complaint or charge filed and/or determination by any Court or administrative agency of illegal discrimination by Contractor.
- 8.7.2.** The HCMA, in its discretion, may consider any illegal discrimination described above as a breach of this Contract and may terminate or cancel this Contract immediately with notice.
- 8.8. Reservation of Rights.** This Contract does not, and is not intended to impair, divest, delegate, or contravene any constitutional, statutory, and/or other legal right, privilege, power, obligation, duty, or immunity of the HCMA.
- 8.9. Force Majeure.** Notwithstanding any other term or provision of this Contract, neither Party shall be liable to the other for any failure of performance hereunder if such failure is due to any cause beyond the reasonable control of that Party and that Party cannot reasonably accommodate or mitigate the effects of any such cause. Such cause shall include, without limitation, acts of God, fire, explosion, vandalism, any law, order, regulation, direction, action, or request of the United States government or of any other government, national emergencies, insurrections, riots, wars, strikes, lockouts, work stoppages, or other labor difficulties. Reasonable notice shall be given to the affected Party of any such event. The Contractor is expected, through insurance or alternative temporary or emergency service arrangements, to continue its obligations under this Contract in the event of a reasonably anticipated, insurable business risk such as business interruption and/or any insurable casualty or loss.
- 8.10. Conflict of Interest.** Pursuant to Public Acts 317 and 318 of 1968, as amended (MCL 15.321, et seq.), no contracts shall be entered into between the HCMA, including all agencies and departments thereof, and any HCMA Agent. To avoid any real or perceived conflict of interest, Contractor shall identify any Contractor Employee or relative of Contractor's Employees who are presently employed by the HCMA. Contractor shall give the HCMA notice if there are any HCMA Agents or relatives of HCMA Agents who are presently employed by Contractor.
- 8.11. Contract Administrator.** Each Party shall designate an employee or agent to act as Contract Administrator. The HCMA's Contract Administrator shall be responsible for such activities as monitoring deliverables and funding addressing the quality of services provided by the Contractor, reviewing invoices and submitting requests to the HCMA's procurement authority for any contract modification. The Contract Administrators for both Parties shall serve as a contact point for all matters related to the services to be performed under this Contract.
- 8.12. Dispute Resolution.** All disputes arising under or relating to the execution, interpretation, performance, or nonperformance of this Contract involving or affecting the Parties may first be submitted to the respective Contract Administrators for possible resolution. The Contract Administrators may promptly meet and confer in an effort to resolve such dispute. If the Contract Administrators cannot resolve the dispute in five (5) business days, the dispute may be submitted to the signatories of this Contract or their successors in office. The signatories of this Contract may meet promptly and confer in an effort to resolve such dispute. Before litigation is commenced by either Party regarding Claims arising under this Contract, the Parties shall use their best efforts to mediate such Claims. All costs for mediation shall be borne equally by the Parties. The Parties shall mutually agree to the mediator.

- 8.13. Access and Records.** The Contractor shall establish and maintain a reasonable accounting system that enables HCMA to readily identify Contractor's assets and Gross Receipts of the Services provided under this Contract, including but not limited to: a full and accurate books of accounts, cash receipts, and other pertinent data customarily used in Contractor's type of operation, showing Contractor's activities under this Contract. The Contractor shall only utilize those recording keeping devices, including without limitation, cash registers, tapes, books, ledgers, journals, sale slips, guest checks, invoices, and cash register maintenance logs which are reasonably acceptable to the HCMA and by which every sale or other transaction related to sundry sales and services are recorded. Contractor will maintain accurate books and records in connection with the Services provided under this Contract for thirty-six (36) months after end of this Contract, and provide the HCMA with reasonable access to such book and records.
- 8.14. Delegation /Subcontract/Assignment/Sublease.** Contractor shall not delegate, assign, sublease or subcontract any obligations or rights under this Contract without the prior written consent of the HCMA.
- 8.14.1.** The rights and obligations under this Contract shall not be diminished in any manner by assignment, delegation, sublease or subcontract.
- 8.14.2.** Any assignment, delegation, sublease or subcontract by Contractor and approved by the HCMA, must include a requirement that the assignee, delegee, or subcontractor will comply with the rights and obligations contained in this Contract.
- 8.14.3.** The Contractor shall remain primarily liable for all work performed by any subcontractors. The Contractor shall remain liable to the HCMA for any obligations under the Contract not completely performed or improperly performed by any Contractor delegee or subcontractor.
- 8.14.4.** Should a Subcontractor fail to provide the established level of service and response, the Contractor shall contract with another agency for these services in a timely manner. Any additional costs associated with securing a competent subcontractor shall be the sole responsibility of the Contractor.
- 8.15.** No provision in this Contract limits, or is intended to limit, in any way the Contractor's right to offer and provide its services to the general public, other business entities, municipalities, or governmental agencies during or after the term of this Contract. This Contract is not an exclusive contract and HCMA may contract with other vendors or contractors to provide the same or similar services at the Facility or other facilities under the jurisdiction of HCMA.
- 8.16. No Implied Waiver.** Absent a written waiver, no act, failure, or delay by a Party to pursue or enforce any right or remedy under this Contract shall constitute a waiver of those rights with regard to any existing or subsequent breach of this Contract. No waiver of any term, condition, or provision of this Contract, whether by conduct or otherwise, in one or more instances, shall be deemed or construed as a continuing waiver of any term, condition, or provision of this Contract. No waiver by either Party shall subsequently affect its right to require strict performance of this Contract.
- 8.17. Severability.** If a court of competent jurisdiction finds a term, condition, or provision of this Contract to be illegal or invalid, then the term, condition, or provision shall be deemed severed from this Contract. All other terms, conditions, and provisions of this Contract shall remain in full force and effect. Notwithstanding the above, if Contractor's promise to indemnify or hold the HCMA harmless is found illegal or invalid, Contractor shall contribute the maximum it is permitted to pay by law toward the payment and satisfaction of any Claims against the HCMA.

**8.18. Captions.** The section and subsection numbers, captions, and any index to such sections and subsections contained in this Contract are intended for the convenience of the reader and are not intended to have any substantive meaning and shall not be interpreted to limit or modify any substantive provisions of this Contract. Any use of the singular or plural number, any reference to the male, female, or neuter genders, and any possessive or nonpossessive use in this Contract shall be deemed the appropriate plurality, gender or possession as the context requires.

**8.19. Notices.** Notices given under this Contract shall be in writing and shall either be personally delivered, sent by express delivery service, certified mail, or first class U.S. mail postage prepaid, and addressed to the person listed below. Notice will be deemed given when one of the following occur: (1) the date of actual receipt; (2) the next business day when notice is sent express delivery service or personal delivery; or (3) three days after mailing first class or certified U.S. mail.

**8.19.1.** If notice is sent to the Concessionaire, it shall be addressed to:

*John Blevins  
Motor City Canoe Rentals, L.L.C  
24500 Goddard Road  
Taylor, MI 48180  
(313) 473-9847*

**8.19.2.** If notice is sent the HCMA, it shall be addressed to:

*Amy McMillan, Director  
Huron-Clinton Metropolitan Authority  
13000 High Ridge Drive  
Brighton, MI 48114-9058  
Tel: (810) 227-2757  
Fax: (810) 225-6212  
Email address: [amy.mcmillan@metroparks.com](mailto:amy.mcmillan@metroparks.com)*

**8.19.3.** Either Party may change the address or individual to which notice is sent by notifying the other party in writing of the change.

**8.20. Contract Modifications or Amendments.** Any modifications, amendments, rescissions, waivers, or releases to this Contract must be in writing, agreed to by both Parties, and added as a change order or amendment to this Contract.

**8.21. Precedence of Documents.** In the event of a conflict between the terms and conditions in any of the documents comprising this Contract, the conflict shall be resolved as follows:

**8.21.1.** The terms and conditions contained in this Contract shall prevail and take precedence over any allegedly conflicting provisions in all other Exhibits or documents.

**8.22. Governing Laws/Consent to Jurisdiction and Venue.** This Contract shall be governed, interpreted and enforced by the laws of the State of Michigan. Except as otherwise required by law or court rule, any action brought to enforce, interpret, or decide any Claim arising under or related to this Contract shall be brought in the 44th Judicial Circuit Court of the State of Michigan (Livingston County), the 53rd District Court of the State of Michigan, or the United States District Court for the Eastern District of Michigan, Southern Division, as dictated by the applicable jurisdiction of the court. Except as otherwise required by law

or court rule, venue is proper in the courts set forth above. The choice of forum set forth above shall not be deemed to preclude the enforcement of any judgment obtained in such forum or taking action under this Contract to enforce such judgment in any appropriate jurisdiction.

**8.23. Entire Contract.** This Contract represents the entire Contract and understanding between the Parties. This Contract supersedes all other prior oral or written understandings, communications, agreements or Contracts between the Parties. The language of this Contract shall be construed as a whole according to its fair meaning, and not construed strictly for or against any Party.

**8.24. HCMA Intellectual Property.** Contractor shall have no copyright, patent, trademark or trade secret rights in HCMA Intellectual Property.

**8.25. Contractor Use of HCMA Servicemark.**

**8.25.1.** The HCMA grants Contractor the non-exclusive right to use its servicemark on publications (in any format) related to or associated with performance of this Contract. Permission to use the servicemark extends to use on the Contractor's website.

**8.25.2.** Contractor shall only use the servicemark as provided by HCMA for the purposes described in this Contract and not for any other purpose.

**8.25.3.** Contractor acknowledges that the HCMA has certain rights in the servicemark and that Contractor has no right, title or interest in the servicemark.

**8.25.4.** The servicemark covered under this Section shall be provided at no cost to Contractor.

**8.25.5.** Contractor's permission to use the servicemark shall cease when the entire Contract is terminated and/or cancelled. Immediately upon termination and/or cancellation of this Contract, Contractor shall not display or depict the servicemark on its website or display, distribute or create any publication (in any format) or display, distribute or create other items that contain the servicemark.

The undersigned executes this Contract on behalf of Contractor and the HCMA, and by doing so legally obligates and binds Contractor and the HCMA to the terms and conditions of this Contract.

**[Signatures on next page]**

**MOTOR CITY CANOE RENTALS L.L.C**

BY: \_\_\_\_\_  
John Blevins

DATE: \_\_\_\_\_

John Blevins, Motor City Canoe Rentals, L.L.C appeared in person before me this day and executed this Contract on behalf of Contractor and acknowledged to me under oath that he has taken all actions and secured any and all necessary approvals and authorizations and has the requisite authority from Contractor to fully and completely obligate and bind Contractor to the terms and conditions of this.

Subscribed and sworn to before me on this \_\_\_\_\_ day of \_\_\_\_\_, 2018.

\_\_\_\_\_  
Notary Public, State of \_\_\_\_\_,

\_\_\_\_\_ County

My Commission Expires: \_\_\_\_\_

Acting in the County of \_\_\_\_\_

**FOR THE HCMA:**

BY: \_\_\_\_\_  
Amy McMillan, Director

DATE: \_\_\_\_\_



**EXHIBIT I**  
**CONTRACTOR INSURANCE REQUIREMENTS**

The Contractor shall provide and maintain, at their expense, all insurance as set forth below, protecting the HCMA against any Claims, as defined in this Contract. The insurance shall be written for not less than any minimum coverage herein specified.

1. **Commercial General Liability** Occurrence Form including: a) Premises and Operations; b) Products and Completed Operations (including On and Off Premises Coverage); c) Personal and Advertising Injury d) Broad Form Property Damage e) Independent Contractors; f) Broad Form Contractual including coverage for obligations assumed in this contract;

\$1,000,000 – Each Occurrence Limit  
\$1,000,000 – Personal & Advertising Injury  
\$1,000,000 – Products & Completed Operations Aggregate Limit  
\$2,000,000 – General Aggregate Limit  
\$ 500,000 – Fire Damage Limit (Any One Fire)

2. **Workers' Compensation** insurance with limits statutorily required by any applicable Federal or State Law and Employers Liability insurance with limits of no less than \$500,000 each accident, \$500,000 disease each employee, and \$500,000 disease policy limit.
3. **Commercial Umbrella/Excess Liability** insurance with a minimum limits of \$3,000,000 each occurrence. Umbrella or Excess Liability coverage shall be no less than following form of primary coverages or broader. The Umbrella/Excess Liability policy must also include and must be in excess of Liquor Liability coverage.
4. **Liquor Liability** insurance with a limit of \$1,000,000 each occurrence; \$1,000,000 annual aggregate.
5. **Commercial Property** insurance. The Contractor shall be responsible for obtaining and maintaining insurance covering their equipment and personal property against all physical damage.
6. **General Insurance Conditions:** The aforementioned insurance shall be endorsed, as applicable, and shall contain the following terms, conditions, and/or endorsements. All certificates of insurance shall provide evidence of compliance with all required terms, conditions and/or endorsements.
  - a. All policies of insurance shall be on a primary, non-contributory basis with any other insurance or self-insurance carried by the HCMA;
  - b. The insurance company(s) issuing the policy(s) shall have no recourse against the HCMA for subrogation, premiums, deductibles, or assessments under any form;
  - c. Any and all deductibles or self-insured retentions shall be assumed by and be at the sole risk of the Contractor;
  - d. All policies, with the exception of Workers' Compensation, shall be endorsed to name the HCMA as additional insured;
  - e. All policies shall be endorsed to provide a written waiver of subrogation in favor of HCMA;
  - f. The Contractor shall require their contractors, or sub-contractors not protected under the Contractors insurance policies, to procure and maintain insurance with coverages, limits, provisions, and/or clauses equal to those required in this Contract;
  - g. Certificates of insurance must be provided no less than ten (10) working days prior to commencement of contract and must bear evidence of all required terms, conditions and endorsements; and
  - h. All insurance carriers must be licensed and approved to do business in the State of Michigan and shall have and maintain a minimum A.M. Best's rating of A.

**HURON-CLINTON METROPOLITAN AUTHORITY**

To: Board of Commissioners  
From: Nina Kelly, Manager of Planning  
Subject: Approval of Willow and Oakwoods Master Plan  
Date: June 7, 2018

**Action Requested: Motion to Approve**

That the Board of Commissioners' approve the Willow and Oakwoods Master Plan as recommended by Manager of Planning Nina Kelly and staff.

**Fiscal Impact:** Projects identified in the Master Plan will affect the Metroparks budget over the 10-year planning horizon, but there is no immediate fiscal impact.

**Background:** The master planning process for Willow and Oakwoods included several opportunities for public input. Three public meetings were facilitated for this plan: the first two were on Saturday mornings in late January and early February 2018 for Willow (at Washago Pond) and Oakwoods (at the Nature Center), respectively. A second meeting was held on a Wednesday evening in late March 2018 for both parks combined at the Oakwoods Nature Center. A total of 79 people signed in for the three meetings. The online and hard-copy questionnaires for each park were available from January through April 2018. Social media posts encouraging people to attend the meetings and to complete the questionnaires were made to the Lower Huron-Willow-Oakwoods Facebook page and Metroparks Twitter account throughout the process, and banners advertising the public meetings were installed at the park entrances.

A total of 277 questionnaire responses were collected for Willow and 209 were collected for Oakwoods.

At the May 10, 2018 Board meeting, the Planning Department presented the draft Willow and Oakwoods Master Plan for review and comment.

The plan draft was posted to [www.metroparks.com/about-us/planning-department](http://www.metroparks.com/about-us/planning-department) through May 25, 2018 for public review and comment.

Planning Department staff compiled comments and made necessary revisions in order to complete the final version of the master plan document presented herein for approval.



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Nina Kelly, Manager of Planning  
 Subject: Project No. 900-18-015  
 Project Title: Bids – Tennis / Basketball Court Rehabilitation  
 Location: Lower Huron and Hudson Mills Metroparks  
 Date: June 7, 2018

Bids Opened: May 23, 2018 at 2:00 p.m.

## Action Requested: Motion to Approve

That the Board of Commissioners' accept the proposal from the sole bidder, Michigan Recreation Construction, Inc., for \$207,180.95 to rehabilitate the basketball courts at Lower Huron and Lake St. Clair Metroparks and the tennis courts at Lower Huron Metropark as recommended by Manager of Planning Nina Kelly and staff.

**Fiscal Impact:** This project is under budget by \$2,819.05. The Board approved 2018 budget included a Major Maintenance project in the amount of \$210,000 for reconstruction of the Lower Huron tennis courts at Woods Creek. As a cost-savings measure and a test case for extending the life of existing pavement, staff is recommending the use of plastic tiled surfacing in lieu of complete reconstruction. This approach will enable staff to also address dilapidated courts at Lake St. Clair while still realizing nominal savings.

**Scope of Work:** Pavement repair and crack filling, installation of tennis court and basketball court site furnishings, installation of plastic tennis and basketball court tiled surfacing, installation of tennis court fence improvements and all incidental construction.

**Background:** During the 2018 budget process, the Lower Huron tennis courts were identified as a priority for reconstruction due to the numerous cracks and heaving and uneven pavement as a result of tree roots and net post footings. The immediately adjacent basketball courts were also incorporated into the project scope, which was estimated at the time to come in at a cost of \$210,000 for reconstruction of the pavement and associated furnishings.

Several courts throughout the Metroparks have been identified as needing either reconstruction or complete removal due to similar levels of deterioration. In an effort to rehabilitate multiple courts with limited financial resources, staff is recommending using, for the first time, plastic tiled surfacing for the basketball courts that are to be retained at Lake St. Clair in addition to the courts at Lower Huron.

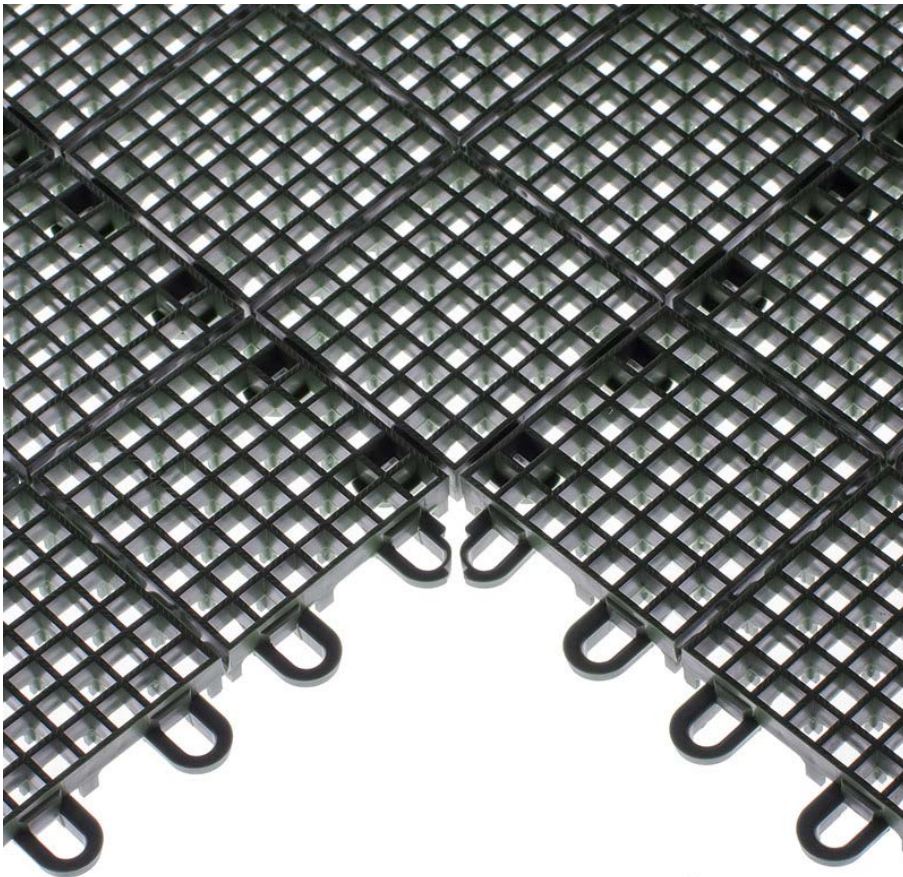
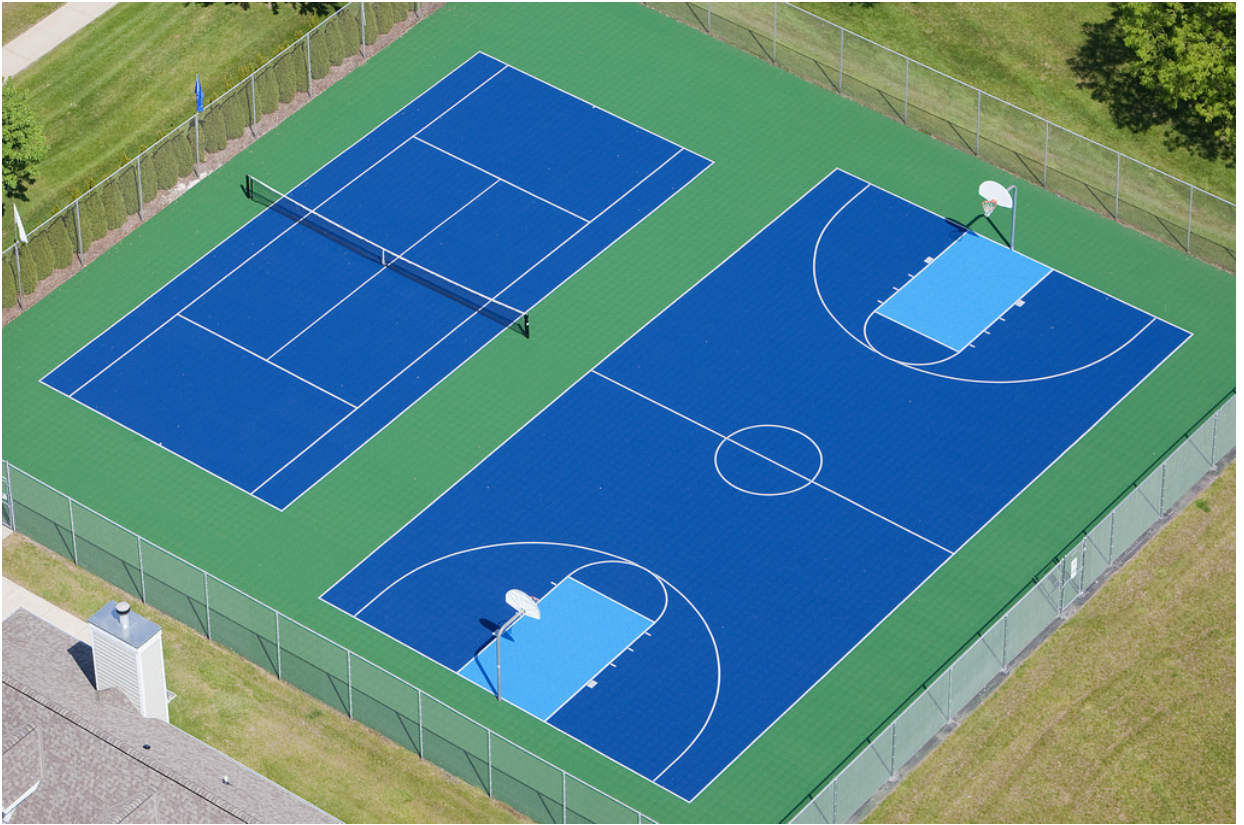
The plastic surfacing material has become popular as a less costly alternative to pavement (whether new or reconstructed) for court games, and is manufactured by several companies with minor differences in product specifications. This project would use Matéflex tiles, preferred by staff due to the purported additional flexibility of an interlocking expansion joint system. The Matéflex product comes with a 15-year limited warranty.

<u>Contractor</u>	<u>City</u>	<u>Amount</u>
Michigan Recreation Construction, Inc.	Howell	\$207,180.95

**Attachments:** Matéflex Example  
 Current Court Conditions



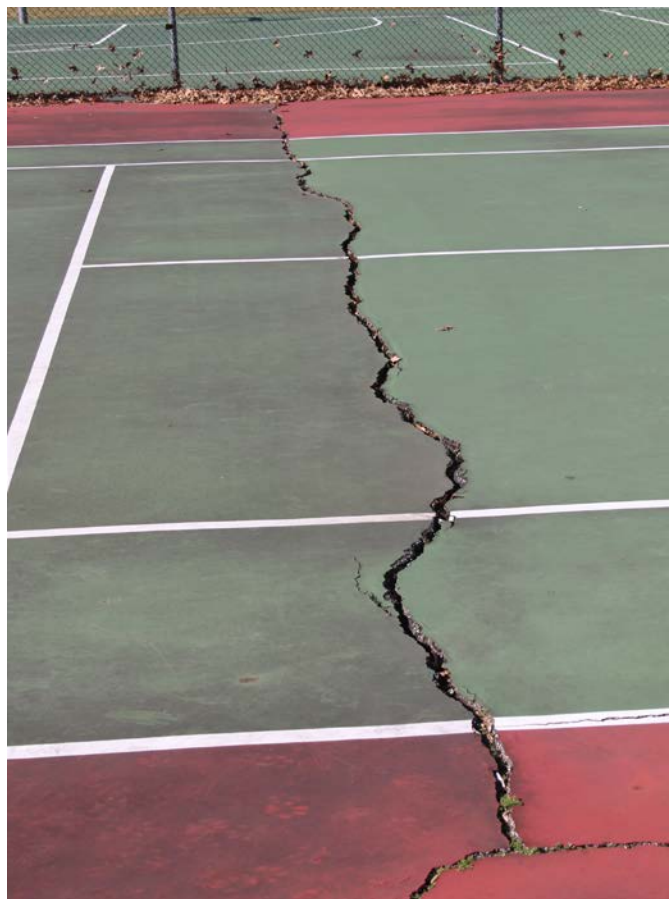
## Matéflex Plastic Tile Surfacing







**Lake St. Clair Basketball Court**



**Lower Huron Tennis and Basketball Courts**



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Manager of Planning Nina Kelly  
 Subject: Approval – Shelden Trails Redevelopment Project Design Services  
 Location: Stony Creek Metropark  
 Date: June 7, 2018

## Action Requested: Motion to Approve

That the Board of Commissioners' approve the proposal submitted by Applied Trails Research LLC in the amount of \$24,410 to facilitate the planning and design phase of the Shelden Trails Development Project as recommended by Manager of Planning Nina Kelly and staff.

**Fiscal Impact:** This was not a budgeted project for 2018. However, the Metroparks Foundation has successfully secured enough grant and other funding to completely cover the estimated cost of the design services from the recommended consultant.

**Scope of Work:** The selected consultant will facilitate the redesign of the Shelden Trails, a multi-use natural surface trail network at Stony Creek Metropark. The redesign is the first step toward implementing the redevelopment project included in the Stony Creek Master Plan approved by the Board in August 2016.

**Background:** The Shelden Trails are used by hikers, mountain bikers, cross country skiers (during the winter season), and several concerns involving user conflicts and a desire for expanded trails were identified during the 2016 master planning process and prior.

The Shelden Trails Development Project is organized into three phases: planning and design, development, and sustainability/maintenance. Should the design begin in June 2018, trail redevelopment and construction would be anticipated for 2019.

### Project Goals

- Create exceptional year-round trail facilities for multiple user groups, including mountain bikers, hikers/runners, fat-tire cyclists (winter) and cross country skiers (winter);
- Improve safety for all trail users;
- Improve trail access and navigability via signage for all trail users;
- Mitigate potential negative impacts to biodiversity areas;
- Increase the number of trail miles;
- Create a defined loop of single-track mountain bike trail; and
- Provide a trail connection between Shelden and Trolley trail systems.

<u>Responding Consultant</u>	<u>City</u>	<u>Proposal Amount</u>
Applied Trails Research LLC	State College, PA	\$24,210
Johnson & Anderson	Waterford, MI	\$35,736
IMBA Trail Solutions	Madison, WI	\$62,520
PEA, Inc.	Brighton, MI	\$65,665

Attachments: Map of Project  
 Applied Trails Research Proposal

# STONY CREEK METROPARK

4300 Main Park Road, Shelby Township, MI 48316 • 586-781-4242

## MAP KEY

- |                 |                          |                         |                            |
|-----------------|--------------------------|-------------------------|----------------------------|
| Rental Shelter  | <b>A</b> Southdale       | <b>F</b> Oakgrove North | <b>K</b> Wintercove North  |
| Restrooms       | <b>B</b> Northdale       | <b>G</b> Ridgewood      | <b>L</b> Wintercove South  |
| Hike-Bike Trail | <b>C</b> Gladeview North | <b>H</b> Lakeview       | <b>M</b> West Branch North |
| Playground      | <b>D</b> Gladeview South | <b>I</b> Baypoint Beach | <b>N</b> West Branch West  |
|                 | <b>E</b> Oakgrove West   | <b>J</b> Mount Vernon   | <b>O</b> West Branch East  |

## PICNIC SHELTERS

## RUSTIC TRAILS

- Pink - 1½ miles
- Blue - 2¼ miles
- Orange - 3½ miles
- Osprey - 1¼ miles
- Shortcuts

## NATURE TRAILS

- East Lake - 3½ miles
- Reflection Trail - ½ mile
- Landscape Trail - 1 mile
- Habitat Trail - 2½ miles
- Shortcuts

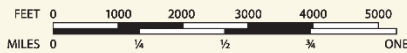
## MOUNTAIN BIKING TRAILS

- Easy - 5½ miles
- More Difficult - 3¼ miles
- Most Difficult - 3½ miles
- Trail Intersection Numbers

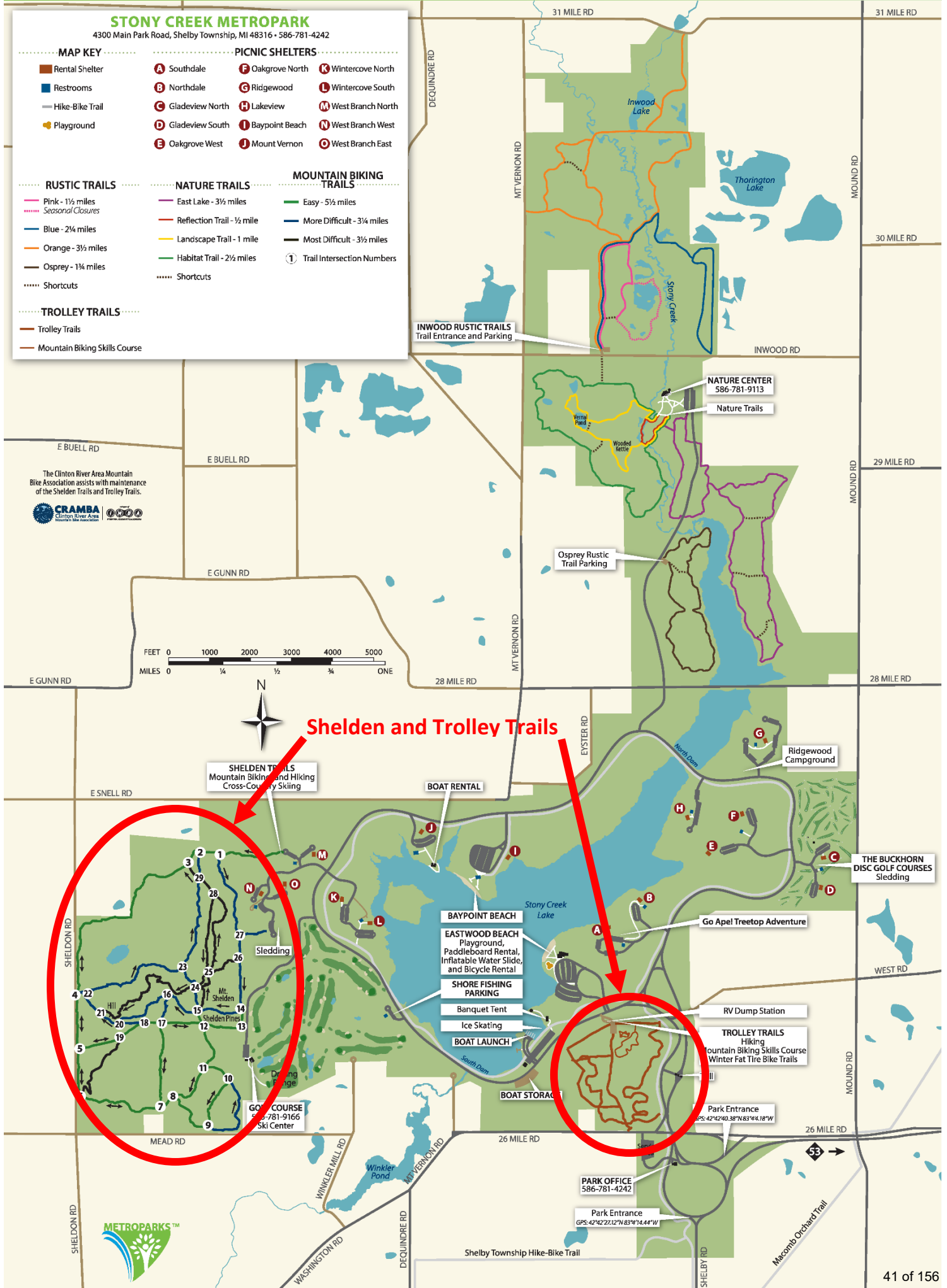
## TROLLEY TRAILS

- Trolley Trails
- Mountain Biking Skills Course

The Clinton River Area Mountain Bike Association assists with maintenance of the Shelden Trails and Trolley Trails.



## Shelden and Trolley Trails







## PART I: TECHNICAL PROPOSAL



### CONTACT INFORMATION

This Proposal is Offered By:

Dr. Jeremy Wimpey  
Applied Trails Research  
1310 N. Allen St.  
State College, PA 16803  
443.629.2630

[jeremyw@appliedtrailsresearch.com](mailto:jeremyw@appliedtrailsresearch.com)



January 4, 2018

Ms. Nina Kelly  
Project Representative  
Clinton-Huron Metropolitan Authority Planning Department  
13000 High Ridge Drive  
Brighton, MI 48114



Dear Ms. Kelly,

Applied Trails Research is privileged to submit this proposal to provide professional services for the Stony Creek Metropark Multi-use, Natural Surface Trail Redevelopment. Our team has a cumulative **30+ years of experience in trails assessment and redevelopment planning.**

The prime contractor for this project will be **Applied Trails Research LLC**, a trails and recreation research company, based in State College, PA, specializing in spatial data collection, analyses and mapping resources for trail planning, design and recreation management. Dr. Jeremy Wimpey, Owner, combines a research background with on-the-ground trail expertise to develop innovative, science-based, adaptive management protocols that balance visitor use with resource protection. His innovations in employing LiDAR technology and GIS analysis in trail planning represent the status of the science and has been employed by the United States Forest Service, Bureau of Land Management, and National Park Service, including multiple trail impact mitigation studies.

**Kay-Linn Enterprises LLC** is a trails, recreation and conservation planning firm based in Boulder, CO. Scott Linnenburger, Principal, brings 20 years of vast experience in natural surface trails planning, assessment, and construction management, ecological restoration of wetland and stream environments, and project management. Linnenburger has provided outreach and education to more than 100 trail volunteer organizations and land management agencies. He was a significant contributor to the International Mountain Bicycling Association trail handbooks and has provided assistance to the United States Forest Service in the recent redevelopments of the agency's Trail Maintenance and Construction Notebook and nationwide Trail Design Parameters.

Our team has worked together over the last seven years to produce **more than 20+ similar trail assessment and redevelopment plans.** It is important to emphasize that our Owner/Operator team members' focus is exclusively on serving our client's needs in the fields of trails and land management planning rather than civil engineering or trails advocacy. We are leaders in the professional field of trail development and have an incredible passion for sustainable land management. We hope you enjoy reviewing our qualifications and we look forward to talking with you further about this project.

Sincerely,

Dr. Jeremy Wimpey  
Owner, Applied Trails Research, LLC

# PROJECT UNDERSTANDING

Applied Trails Research and Kay-Linn have enjoyed successful partnerships due to each company's respect for context-sensitive and community-focused projects. Balancing natural resource function, public safety, and providing high-quality, diverse recreation opportunities for reflection, physical activity, and appreciation of the conserved resources is a challenge our team engages on multiple levels.

We approach a project's sustainability from not just a physical trail sustainability perspective, but with equal weight given to sustainability from managerial and social perspectives. The ultimate trail system derived from this contextual process is successful only when these spheres of sustainability intertwine and function in a collective manner.

While the suite of needs at Stony Creek will be challenging (see Past Performance for similar, successful projects) to fulfill, we firmly believe that the Sheldon and Trolley trails can be redeveloped as a model for sustainable, four-season trail-based recreation.

## TRAIL INVENTORY AND ASSESSMENT:

- **Kick-Off Meeting and Project Management – June, 2018**

Client meeting following award and contracting. Facilitation of transfer of GIS and other background information for project, discussion, and adjustment to timelines as needed by Huron-Clinton Metropolitan Authority (HCMA). This is based on HCMA awarding this project in June 2018.

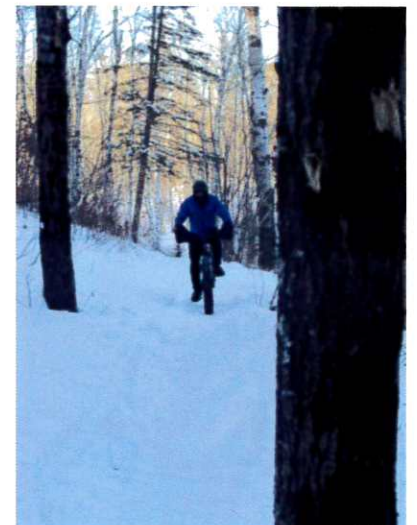
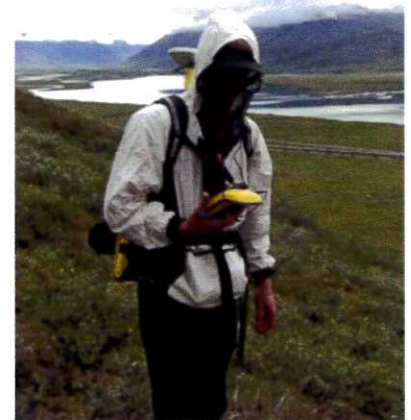
- **Current Condition Assessment – July, 2018**

Team will conduct a field review and inventory of existing official and unofficial trails at Stony Creek. Our team plans to coordinate Stakeholder Engagement #1 and additional managerial outreach and interface into this week-long mobilization. This will control costs, and add to efficiency of our approach.

- **Stakeholder/User Community Engagement #1 – July, 2018**

Team will engage various user communities and interface with individual and groups of users to better understand the desires, issues and concerns of the trail system's user populations. Outreach will also seek to collect relevant managerial and regulatory guidance related to the trail systems, land management and desired future conditions from the agency and partners' perspectives.

- **Optional Deliverable-** Team can provide a 2-day, comprehensive sustainable trail (physical, social, and managerial) development and management seminar for stakeholders/managers to develop common language, goals, and expectation for the redevelopment of the Stony Creek Metropark trail systems.





# PROJECT UNDERSTANDING

## TRAIL SYSTEM EVALUATION AND REDEVELOPMENT PLANNING:

- **Conceptual Redevelopment Plan – September, 2018**

Team will synthesize information gathered during field inventories, user community engagement and managerial outreach to evaluate the current trail system(s) against regulatory, managerial and sustainability thresholds at multiple scales. These conditions and recommendations will be developed into narrative and map-based planning products for review by HCMA.

- **Stakeholder/User Community Engagement #2 – mid-October, 2018**

The team will make a presentation of the assessment and conceptual trail system redevelopment to stakeholders. This is scheduled for six weeks after the delivery of draft materials to allow time for HCMA review and comments to be addressed prior to public engagement.

- **Redevelopment Recommendations - mid-November, 2018**

Following stakeholder and HCMA feedback and comment, our team will work to revise the Redevelopment Plan into a final report with associated recommendations for next steps.

- **Optional Deliverable-** Field design (and cost estimates) of new and/or relocated trails to be discussed after quantities can be estimated via the assessment.



## APPENDICES AND SUPPLEMENTAL MATERIALS:

- Relevant scientific and practical literature specific to issues and opportunities present within Sheldon and Trolley trails
- Maps of current conditions and recommendations (.pdf and .mxd datasets)
- GIS data documenting current trail conditions and recommendations (.shp format)
- Sign inventory data to include photographs and GIS data of sign locations (geo-tagged photos and .shp format)





# PAST PERFORMANCE

## DULUTH TRAVERSE TRAIL SYSTEM MINI MASTER PLAN

**Client:** City of Duluth Parks and Recreation

**Year Completed:** 2017

**Scope:** Kay-Linn and Applied Trails Research, assisting Confluence Design, provided Master Planning services for the Duluth Traverse, a multi-use trail system linking neighborhoods and people to green spaces and our parks. It is a singletrack, natural surface trail, optimized for four season mountain bike and trail running use. It will be the first 100-mile trail system of its kind, wholly within an urban environment- a shared vision between The City of Duluth and the Cyclists of Gitchee Gumees Shores (COGGS).

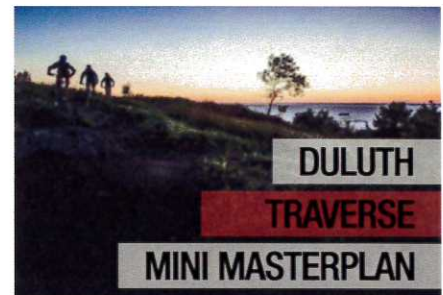
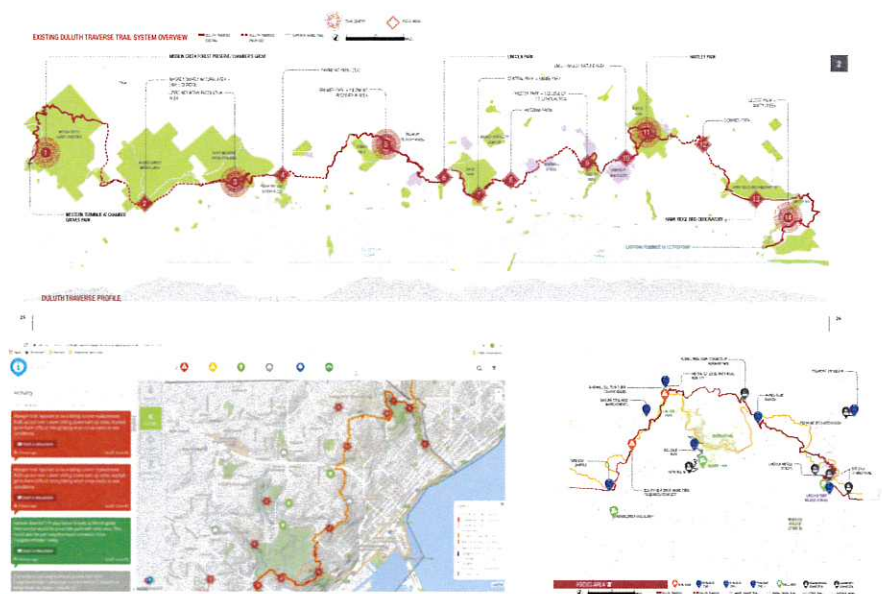
The team of mountain bike and natural trail surface design experts to analyze the built trail sections, plans for future segments, associated infrastructure needs, neighborhood connection opportunities, and natural resource protection elements that will make this trail system and national model for long-term sustainability. Extensive public engagement and use of an online, interactive social engagement tool facilitated robust public discussion, networking, and community building among trail user groups. Our team integrated photography into a graphically rich master plan document that celebrates mountain biking, the City of Duluth's landscapes, parks, and people, and a volunteer and staff-engaged formalized maintenance reporting and response system.



## REFERENCE

**Mr. Jim Shoberg**  
Project Manager  
Duluth Parks & Recreation  
411 West First Street  
Duluth, Minnesota 55802

Phone: 218.730.4316  
email: [jshoberg@DuluthMN.gov](mailto:jshoberg@DuluthMN.gov)



CONFLUENCE DESIGN PART 1: MASTER PLAN 2017





# PAST PERFORMANCE

## HAMILTON COUNTY, OHIO NATURE TRAIL ASSESSMENT AND DEVELOPMENT PLAN

**Client:** Great Parks of Hamilton County

**Year Completed:** 2017

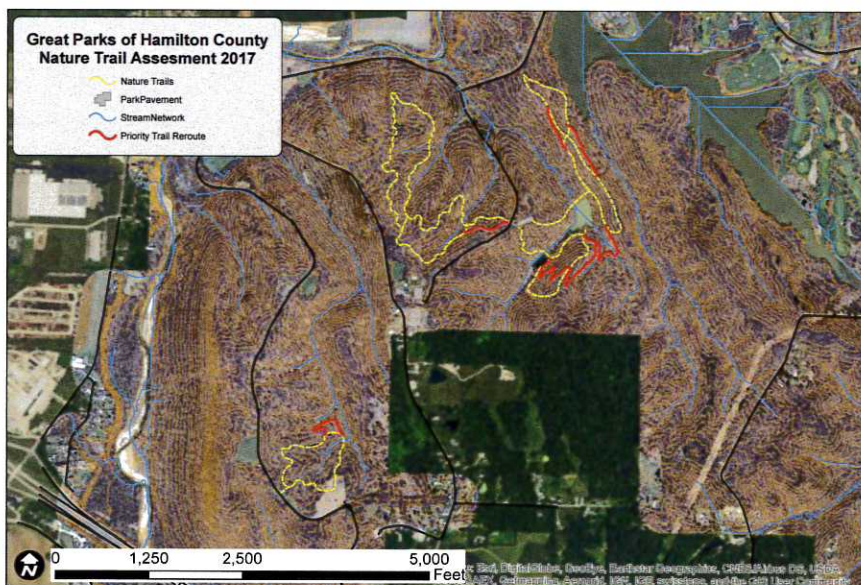
**Scope:** Applied Trails Research, working with Kay-Linn, provided a natural surface trail assessment for Great Parks of Hamilton County, spanning 34 miles of trails in Fernbank, Glenwood Gardens, Miami Whitewater Forest, Mitchell Forest, Sharon Woods, Shawnee Lookout, Winton Woods, Withrow, and Woodland Mound Parks. The project included risk management, signage, accessibility, physical sustainability, and maintenance aspects to the assessment and recommendations for trail system retrofit design, maintenance prescription, construction documentation/specification development, phasing, and cost-estimating. The work also included recommendations for trail system expansion to spread use and improve recreation opportunities in high-profile parks. The project provided numerous accessibility opportunities, improved development of mountain bike trail facilities, and long-term maintenance improvements that will conserve management resources while mitigating resource impacts.



## REFERENCE

Mr. Ben Braeutigam  
Natural Resources Manager  
5401 Zion Road  
Miami Township, OH 45002

Phone: 513.941.9034  
email: [bbraeutigam@greatparks.org](mailto:bbraeutigam@greatparks.org)





# PAST PERFORMANCE

## OAK OPENINGS MOUNTAIN BIKE TRAIL MASTER PLANNING, FIELD DESIGN, AND DEVELOPMENT PLAN

**Client:** MetroParks Toledo

**Year Completed:** 2016

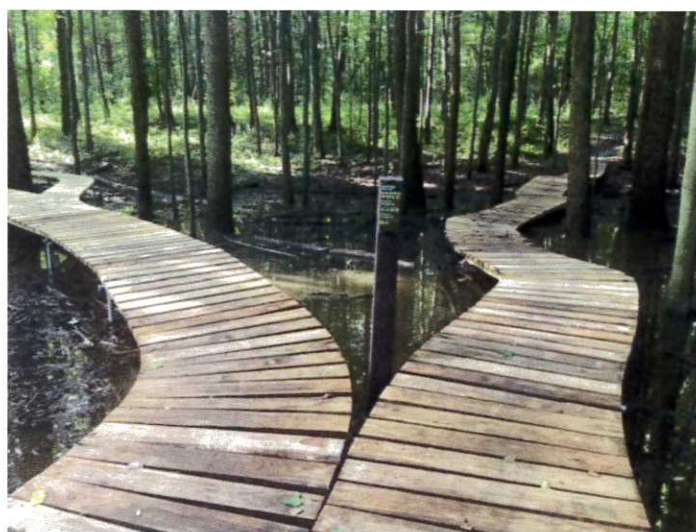
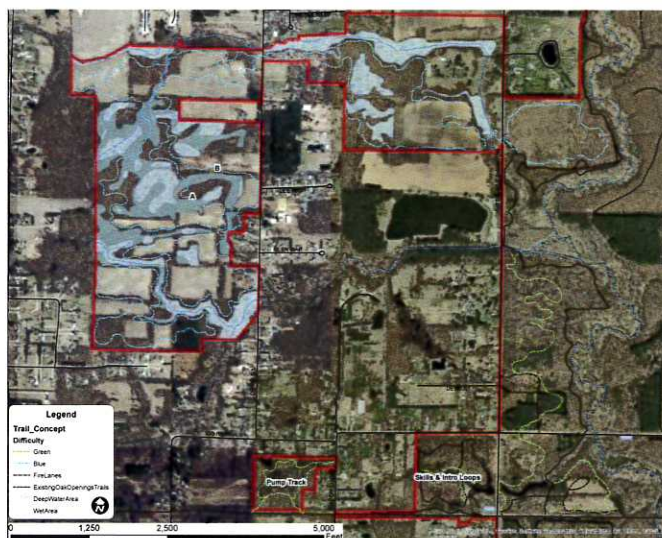
**Scope:** Kay-Linn Enterprises, working with Applied Trails Research, provided a terrain assessment, public education and conceptual design charette, conceptual planning, and detailed field design and construction specification development for a 15-mile mountain biking trail system (the first in MetroParks Toledo) in the largest and most significantly biodiverse park in the MetroParks system. Designed for hybrid construction development by MetroParks staff and local volunteers, avoiding sensitive wetland areas, the project has led to increased park visitation, higher engagement of families in active outdoor recreation, and the need to redevelop a new 10-acre park property into a family-focused trailhead, bike park, picnic, and treehouse camping area connected to the trail system with assistance from Kay-Linn and Treehouse Masters. The project's education component has also improved the trail construction acumen of MetroParks staff and local volunteers, spreading improved maintenance practices to additional park units.



## REFERENCE

**Mr. Joe Fausnaugh**  
Chief of Operations  
6101 Fallen Timbers Lane  
Maumee, Ohio 43537

Phone: 419.407.9744  
email:  
[joe.fausnaugh@metroparkstoledo.com](mailto:joe.fausnaugh@metroparkstoledo.com)





# PAST PERFORMANCE

## HAMPTON HILLS TRAIL ASSESSMENT, REDEVELOPMENT PLAN, AND TRAIL STAFF TRAINING

**Client:** Summit Metro Parks, Ohio

**Year Completed:** 2015

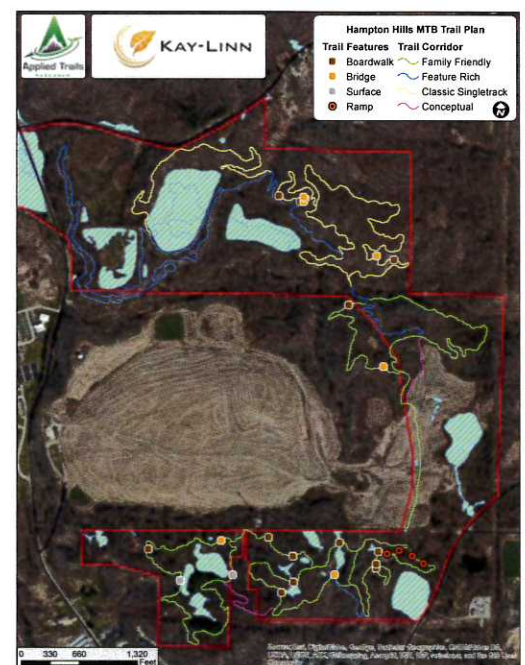
**Scope:** Applied Trails Research, working with Kay-Linn, provided a terrain assessment, regional natural surface trail training, trail system planning, design, staff training, and construction management for the first mountain bike trail system in the Summit Metro Parks Department around Akron, Ohio. The resulting 12-mile mountain bike-optimized trail system includes the progressive offerings needed by first-time mountain bikers through wooden ladder drops and flow trails that satisfy experienced and talented riders. The ATR/K-L team also engaged the assistance of professional trail contractors Dirt Artisans, Ironwood Outdoors, and Valerie Naylor to augment staff education on mechanized machinery and provide productivity recommendations in trail maintenance operations. The resulting mountain bike trail system is one of the highest lauded trail areas in Ohio.



## REFERENCE

**Mr. Mark Avery**  
Operations Project Manager

**Phone:** 330.475.1462  
**email:** [Mavery@summitmetroparks.org](mailto:Mavery@summitmetroparks.org)





# PAST PERFORMANCE

## CASPER MOUNTAIN RECREATION MASTER PLAN

**Client:** Natrona County, WY

**Year Completed:** 2014

**Scope:** Kay-Linn Enterprises, working with Applied Trails Research, provided a recreation assessment, connectivity, and development plan for Natrona County's recreational holdings on Casper Mountain, the mountain backdrop for the City of Casper.

With six conservation properties interspersed in a patchwork assemblage of private properties, camps, a City of Casper downhill ski area, and BLM lands on the 10,000-acre Casper Mountain, the project assessed and provided retrofit concepts for the County's backcountry singletrack, groomed nordic ski, accessible/sight-impaired, frontcountry aggregate surfaced, field archery range, and campground/facility access trails and related facilities. The plan will guide capital investments and connectivity improvements in the County's endeavor to provide a world-class, interconnected, diverse trail system for residents, visitors, and events.

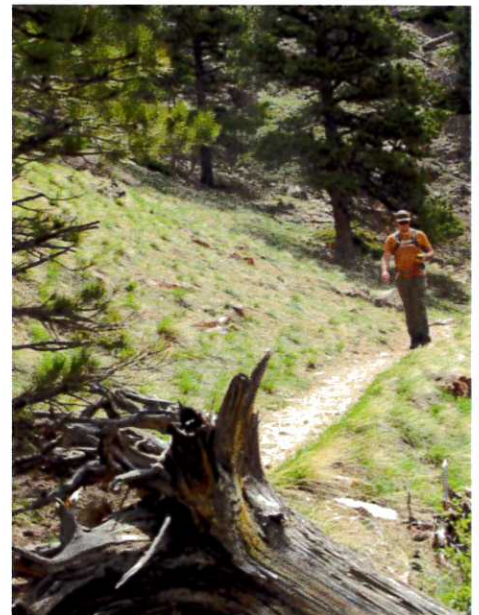
Kay-Linn conducted stakeholder outreach with Natrona County and all the represented recreation groups, nearby property owners, the City of Casper, provided public presentation and discussion of the potential impacts of the plan's implementation, and began the process of partnership alignment to achieve connectivity across the private-public land interface.



## REFERENCE

Mr. Richard O'Hearn  
Natrona County Parks Director  
538 SW Wyo Blvd.  
Mills, WY 82644

Phone: 307.235.9325  
email: [rohearn@natronacounty-wy.gov](mailto:rohearn@natronacounty-wy.gov)





# PAST PERFORMANCE

## CITY OF DES MOINES NATURE TRAIL ASSESSMENT AND DEVELOPMENT PLAN

**Client:** Des Moines Parks and Recreation Department

**Year Completed:** 2013



**Scope:** Kay-Linn Enterprises, working with Applied Trails Research, provided a recreational trail assessment for Des Moines Parks and Recreation spanning trails in Ewing, Greenwood/Ashworth, Grandview, and Beaverdale Parks. The project included assessment of Ewing and Ashworth Parks' 8 miles of shared-use trails, trail system retrofit design, construction documentation/specification development, and cost-estimating. Ewing Park's mountain bike-focused project includes beginner skills area development, cross country singletrack, and intermediate-level flow trails, as well as advanced technical trail features. Ashworth Park's shared-use trail project focused on the redevelopment of unsustainable trails, retrofit on existing trails to a navigable system, and reducing trail density to better conserve sensitive forest resources. Grandview and Beaverdale Parks' assessments focused on the feasibility design of accessible, natural surface trails to improve universal access in neighborhood recreational trails.

## REFERENCE

**Mr. Richard Brown**  
Park Planner

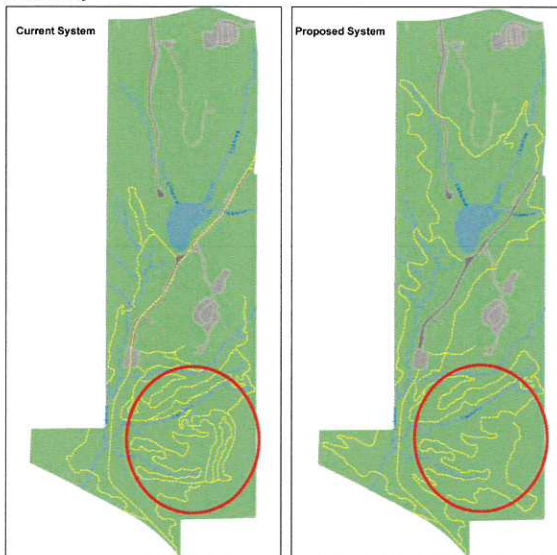
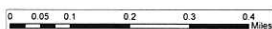
**Phone:** 515.248.6356

**email:** rrbrown@dmgov.org

### Ashworth and Greenwood Parks: Trail Density

Comparison of current trail system to proposed trail system at Ashworth and Greenwood Parks.  
Note that trail mileage has been increased but dispersed throughout both parks.

This dispersal reduces trail density in the area of the park with exceptional conservation value (red oval indicates approximate location).



### Ewing Park: Conceptual Trail Plan



# PAST PERFORMANCE

## CHATTAHOOCHEE-OCONEE NATIONAL FOREST TRAIL ASSESSMENT AND REDEVELOPMENT PLAN

**Client:** United States Department of Agriculture, United States Forest Service (USFS): Chattahoochee-Oconee National Forest

**Year Completed:** 2012

**Scope:** Applied Trails Research, teaming with Kay-Linn Enterprises and Trail Dynamics, provided field-based trail assessment and spatial analysis products for more than 220 miles of multi-use, natural surface trails throughout the Chattahoochee-Oconee National Forest. Applied Trails Research coordinated spatial data collection and analysis, including a sign and engineered structure inventory. Kay-Linn and Trail Dynamics melded the spatial information with field-determined physical, social, and managerial setting assessment to provide a comprehensive set of recommendations that provides the USFS with solutions to problems and guidance for resource protection and visitor experience management across the Forest. The project also included extensive volunteer outreach and education, with a weekend-long trails education seminar, three field-based education sessions, and four on-the-ground demonstration projects.

**Outcomes:** The final plan will become a key facet of a collaborative partnership between USFS and non-profit partners (Co-Trails) to plan for a system-wide strategic plan to:

- 1) improve the condition of existing trails,
- 2) enhance maintenance protocols of existing trails, and
- 3) Raise and coordinate the knowledge base of trail group volunteers and Forest Service staff



## REFERENCE

John W. Campbell  
Recreation, Engineering and Lands  
Staff Officer  
Chattahoochee-Oconee NF's  
1755 Cleveland Hwy  
Gainesville, GA 30501

770.297.3033 (o)  
Email: [jwcampbell@fs.fed.us](mailto:jwcampbell@fs.fed.us)





# PAST PERFORMANCE

## CAVE RUN LAKE NATURAL SURFACE TRAIL ASSESSMENT AND REDEVELOPMENT PLAN

**Client:** United States Department of Agriculture, United States Forest Service (USFS): Daniel Boone National Forest

**Year Completed:** 2011

**Scope:** Applied Trails Research and Kay-Linn Enterprises assisted Trail Dynamics, providing field-based trail assessment and spatial analysis products for a more than 90 miles of non-motorized system and proposed system trails around Cave Run Lake, near Morehead, Kentucky. Applied Trails Research developed spatial analysis products to drive the inventory and assessment. The team coordinated on the field-based assessment that covered (by boot and bike) approximately 100 miles of trail and compiled the comprehensive solutions-based recommendations, including site-specific Trail Design Parameters, maintenance and equipment protocols, and trail stewardship strategies in support of ongoing NEPA-related planning.

**Outcomes:** The final plan is facilitating USFS and non-profit partners to decide the future of the Cave Run Lake trails, based on comprehensive, empirical data. The document has been employed to:

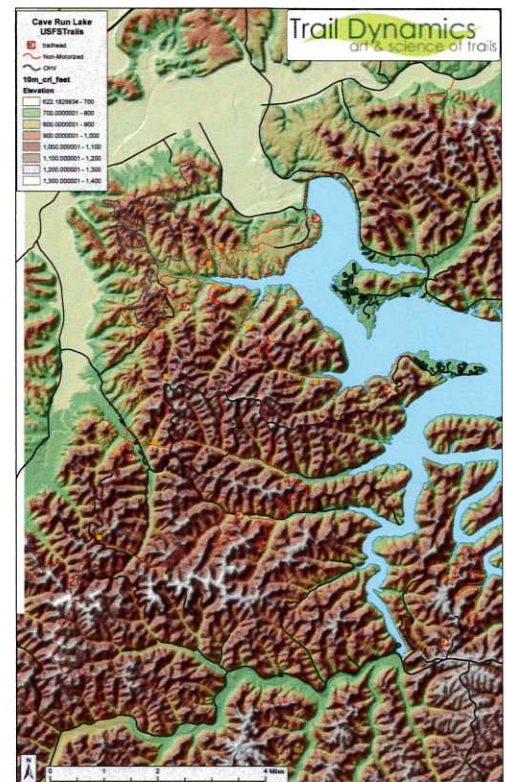
- 1) develop alternatives for the Environmental Assessment,
- 2) enhance maintenance protocols of existing trails,
- 3) develop a more active stewardship program for the upkeep and public outreach around the area's trails.
- 4) efficiently deploy staff and funding toward the highest priority projects,
- 5) direct the development and phasing of a new 35-mile shared-use, natural surface trail system connected to the City of Morehead.



## REFERENCE

**Becky Riegle**  
Environmental Coordinator  
TEAMS Enterprise Unit  
USDA Forest Service

[rriegle@fs.fed.us](mailto:rriegle@fs.fed.us)  
989.724.9381



## ADDITIONAL EXPERIENCE

### **Applied Trails Research, LLC**

#### **10 years in the field of trails and recreation-related geospatial research and assessment**

- Chattahoochee National River & Recreation Area (GA) Trail System Assessment and Redevelopment Plan, 2018
- Bull Run/Occoquan Trail (VA) Trail Redevelopment Plan and Design, Ongoing
- Montpelier (James Madison Estate) Trail System Design and Development Assistance, Ongoing
- Wayne National Forest (OH) Destination Trail System Planning and Design, 2017
- Pacific Crest National Scenic Trail Assessment, 2016 - Present
- Rothrock State Forest (PA) Trail Assessment, 2017 - Present
- Smith Rock State Park (OR) Trail Assessment, 2017
- US Virgin Islands National Park, Lind Point Trail Redevelopment Plan, 2016
- Bob Creek Wildlands (Alberta) OHV Trail Assessment and Redevelopment Plan, 2017
- Michaux State Forest (PA) Trail Assessment, 2016
- Spirit Mountain Bike Park All-Weather & Beginner Downhill Trail Design, 2016
- Grand Staircase-Escalante National Monument Campsite Impact Assessment, 2016 - Present
- Appalachian National Scenic Trail Assessment, 2015-2017
- Lake County MetroParks (OH) Natural Surface Trail Feasibility Assessment, 2015
- Whissenhunt (GA, USFS) OHV Trail Assessment & Redevelopment Plan, 2015
- Patuxent River State Park-North (MD) Trail Assessment & Redevelopment Plan, 2014
- Community Trail Plan: Coldwater Mountain (AL) Destination Trail System, 2011

### **Kay-Linn Enterprises, LLC**

#### **20 years in the field of trails, open space, and conservation planning**

- Swift Creek MTB Trails (Richmond, VA IMBA Ride Center) Risk Assessment, 2017
- Great Passion Play (AR) Mountain Bike Trail System Design, 2017
- Michigans Edge Mountain Bike Association (MI) Trail System Fundraising Plan, 2017
- Pole Mountain Trail System Redevelopment Design (Medicine Bow National Forest, WY), 2017
- Schoolyard Mountain Bike Trail System Design (Laramie, WY), 2017
- Welch Village Ski and Snowboard Area (MN) Bike Park Master Plan, 2016
- Soldier Mountain Ski Area (ID) Bike Park Master Plan, 2016
- Jakes Rocks Destination Trail System (PA) Design, Permitting, and Construction Management, 2015 - Present
- South Canyon Mountain Bike Trail System (CO) Fundraising, 2016
- Grand County Strategic Trails Plan, (Grand County, CO), 2015, 2018
- Whiterock Conservancy Destination Trail System Planning, Design, and Construction Management, 2013 - 2015
- Avalanche Preserve (MI) MTB Trail Development Plan, 2015
- South Canyon (CO) Trail Development Plan, 2015
- Indian Trail- Wyalusing State Park (WI) Trail Redevelopment, 2014
- Granby Ranch (CO) Bike Park and Community Trail System Development Plan, 2014
- MOMBA Trails (OH)- Family-Friendly Bike Skills Park Design, 2014
- Beck Lake Bike Park (Cody, WY) Planning and Development Assistance, 2014
- Jersey Valley Park (WI) Trail Assessment and Development Plan, 2014
- Percy Warner Park (TN) Mountain Bike Park Trail Design and Project Management, 2013
- Overland Mountain Bike Club (CO) Trail Vision Plan, 2013



# ORGANIZATION

## APPLIED TRAILS RESEARCH

### Contact Information:

Dr. Jeremy Wimpey, Owner  
PO Box 10304  
State College, PA 16805  
443.629.2630  
appliedtrailresearch@gmail.com  
www.appliedtrailsresearch.com

### Company Description

Applied Trails Research, LLC serves trail professionals and government leaders with geospatial consulting services. Applied Trails Research is a member of the Professional Trail Builders Association (PTBA). Licensed and insured, we offer a variety of services to those in the trail industry. Applied Trails Research specializes in geographic data collection and analysis, and production of maps for trail planning, design and recreation management. Applied Trails Research, LLC also works with federal, state and local land management agencies to provide assistance with development of science-based best management practices that balance visitor use with resource protection.

### Key Staff- Dr. Jeremy Wimpey

Jeremy Wimpey, PhD. is the sole proprietor of Applied Trails Research, LLC. Dr. Wimpey has over 12 years of professional and academic experience in the field of trail and recreation-related spatial analysis. His unique background—academic, trail enthusiast, entrepreneur—help bring an innovative and holistic approach to providing solutions for recreation management challenges. With a doctorate in geospatial environmental analysis from Virginia Tech, Jeremy's academic background and facts-first approach allow him to apply science and research to solve resource and experiential challenges.

A member of the Professional Trailbuilder's Association (PTBA) and the Association of American Geographers (AAG), Dr. Wimpey has collaborated with the National Park Service and US Forest Service, supported the work of research institutions and provided geospatial analysis for private park development in the field of recreational ecology. Jeremy has published numerous trail impact and trail management works in academic journals and popular sources. In addition to publications, Jeremy has focused extensive efforts on public education of trail users as a lecturer and volunteer. A native of Virginia, Jeremy received his BS and MS in Geography at James Madison University.



### DIVISION OF SCOPE

- Project Management
- Lead, Trail Assessment, Signage/Wayfinding
- Support, Stakeholder Outreach, Redevelopment Plan



# TEAM RESUMES



## JEREMY WIMPEY

1310 North Allen St. State College, PA 16803 | 443.629.2630 | [appliedtrailsresearch@gmail.com](mailto:appliedtrailsresearch@gmail.com)

### EDUCATION

Virginia Tech, Blacksburg VA  
College of Natural Resources, Departments of Geography and Forest Resources & Environmental Conservation  
**PhD in Geospatial Environmental Analysis (GAEA)** 2009  
Dissertation: "Assessing and Evaluating Recreational Trails on Public Lands"

James Madison University, Harrisonburg, VA  
College of Integrated Science and Technology  
**M.S. in Integrates Science and Technology (ISAT)** 2006  
Thesis: "GIS-based predictions of trail sustainability: slope analysis as a primary indicator"

James Madison University, Harrisonburg, VA  
**B.S. in Geography** 1999

### TEACHING EXPERIENCE

**Teaching Assistant- to Dr. Karen Hockett in "Outdoor Recreation Planning"** Fall 2008  
Guest Lecturer, grading, prepared class materials

**Teaching Assistant- to Dr. Steven Lawson in "Outdoor Recreation Planning"** Fall 2006 and 2007  
Guest Lecturer, grading, prepared class materials

**Teaching Assistant- to Dr. Glen Gustafson "Physical Geography, Intro and Advanced Remote Sensing"** Fall 2003- Spring 2005  
Lab Instructor, grading, prepared class materials

### PAPERS AND REPORTS

Wimpey, Jeremy, and Jeffrey L. Marion. *Formal and Informal Trail Monitoring Protocols and Baseline Conditions: Great Falls Park and Potomac Gorge. Virginia Tech, Department of Forestry and Environmental Conservation Final Research Report, Blacksburg, VA.* 2010

Wimpey, J., Marion, J.L. *A Spatial Exploration of Informal Trail Networks Within Great Falls Park, Virginia. Journal of Environmental Management, Accepted, in press.* 2010

Wimpey, J., Marion, J.L. *The Influence of Use, Environmental and Managerial Factors on the Width of Recreational Trails. Journal of Environmental Management 91(10): 2028-2037.* 2010

Monz, C. A., Marion, J. L., Goonan, K. A., Manning, R.E., Wimpey, J., Carr, C. *Assessment and monitoring of recreation impacts and resource conditions on mountain summits: Examples from the Northern Forest, USA. Mountain Research and Development 30(4), Accepted, in press.* 2010

Marion, Jeffrey L., Jeremy Wimpey, and Logan Park. *Informal and Formal Trail Monitoring Protocols and Baseline Conditions: Acadia National Park. Draft Final Management Report. Virginia Tech College of Natural Resources, Forestry/Recreation Resources Management, Blacksburg, VA.* 2010

Marion, Jeffrey L., Jeremy Wimpey, and Logan O. Park. "Monitoring and assessing trail conditions at Acadia National Park" Book Chapter In: R.E. Manning (ed.), pp. 185-195. *Parks and People: Managing Outdoor Recreation at Acadia National Park.* Burlington, VT: University of Vermont Press. 2009

Marion, Jeffrey L. and Jeremy Wimpey. "Environmental impacts of mountain biking: Science review and best practices" Book Chapter In: *Managing Mountain Biking.* Pete Webber, editor. International Mountain Biking Association, Boulder, CO pp. 94-111. 2007

### Contributor to:

Marion, Jeffrey L. and Chris Carr. *"Backcountry Recreation Site and Trail Conditions: Haleakalā National Park"* Virginia Tech, Final Research Report, Blacksburg, VA. 2009



## TEAM RESUMES



Hockett, K., A. Clark, Y.F. Leung, J.L. Marion, L. Park, J. Wimpey, & C. Widman. "Deterring Off-Trail Hiking in Protected Natural Areas: Evaluating Options with Surveys and Unobtrusive Observation." Virginia Tech, Final Research Report, Blacksburg, VA.

### Reports In progress:

Wimpey, Jeremy & Jeffrey L. Marion. *Informal Trail and Visitor Experience Protocols: Denali National Park.* Virginia Tech, Department of Forestry and Environmental Conservation, Final Research Report, Blacksburg, VA.

Marion, Jeffrey L., Jeremy Wimpey, & Logan Park. *Vegetation and Soil Monitoring Protocol Development and Baseline Conditions: Acadia National Park.* Virginia Tech, Department of Forestry and Environmental Conservation, Final Research Report, Blacksburg, VA.

### PRESENTATIONS

#### Invited Presentations:

- |  |      |
|--|------|
| "Trail Science, Design and Management"   | 2010 |
| Presentation and Panel Member USFS and IMBA Tahoe Trails Conference, South Lake Tahoe, October 1-3 <sup>rd</sup> |      |
| "Environmental Impacts of Mountain Biking: Science Review and Best Practices"                                    | 2010 |
| Presentation at International Mountain Bike Association World Summit, Augusta, GA                                |      |
| "Recreation Ecology: The Science Behind Sustainable Recreation"  | 2009 |
| Presentation at Professional Trail Builders Association annual meeting Asheville, North Carolina                 |      |
| "Environmental Impacts of Mountain Biking: Science Review and Best Practices"                                    | 2008 |
| Presentation at International Mountain Bike Association World Summit Park City, Utah                             |      |
| "Recreation Ecology Primer: The Science Behind Sustainable Recreation"   | 2007 |
| Presentation at Professional Trail Builders Association annual meeting Reno, Nevada                              |      |

#### Professional Conference Presentations:

- |   |      |
|---|------|
| "Informal Trail Assessment and Empirical Modeling of Spatial Distribution and Density: Insights and Discussion" | 2009 |
| Presentation at George Wright Society Meeting Portland, Oregon  |      |
| "Monitoring Trampling Impacts on a Mountain Summit with Ground-based Digital Photography and Image Processing"  | 2009 |
| Presentation at American Association of Geographers Annual Meeting Las Vegas, Nevada                            |      |
| "Monitoring Informal Trails of Potomac Gorge"   | 2009 |
| Presentation at informal trails workshop held at Great Falls Park, Virginia                                     |      |
| "Monitoring Informal Trail Networks in Parks: Some Methods and Results"   | 2008 |
| Presentation at South East Recreation Research Conference, Savannah, Georgia                                    |      |
| "Monitoring Informal Trail Networks in Parks: Some Methods and Results"   | 2008 |
| Presentation at International Symposium on Society and Resource Management Burlington, Vermont                  |      |
| "Methodological Approaches for Monitoring the Number, Spatial Extent, and Condition of Informal Trails"         | 2007 |
| Presentation at George Wright Society Meeting Minneapolis, Minnesota  |      |

### LANGUAGES

English- Native language  
French Basic Competency (read write)

### MEMBERSHIPS

American Association of Geographers (AAG)  
Professional Trail Builders Association (PTBA)

# ASSESSMENT TEAM

## KAY-LINN ENTERPRISES, LLC

### Contact Information:

Scott Linnenburger, Principal  
100 S. 36th Street  
Boulder, CO 80305  
303.241.3301  
scott@kay-linn.com  
www.kay-linn.com

### Company Description

Kay-Linn Enterprises is a small consulting firm that provides comprehensive and sustainable recreation and conservation planning services for communities, organizations and coalitions. With broad experience in sustainable recreation development, trail and environmental education, habitat conservation planning, environmental restoration, and fundraising, Kay-Linn has a multi-faceted understanding of community process, recreation planning, open space management, funding development, and ultimately, successful and sustainable project implementation.

Kay-Linn Enterprises is a member of the Professional Trailbuilders Association and Linnenburger currently holds a position on the Board of American Trails.

### Key Staff- Mr. Scott Linnenburger

Scott Linnenburger has more than a fifteen years of experience in recreation/trail development, conservation planning, and environmental restoration projects. His successful work on more than 100 trail projects in the federal, state, local, and private sectors demonstrates a keen ability to assist clients in developing projects that fulfill their mission, protect natural resources, and enhance community sustainability. His simultaneous commitment to collaboration and technical expertise has allowed him to play multiple professional roles, from field scientist to project manager, professional trails educator to program director.

While working with the International Mountain Bicycling Association, Linnenburger developed the Trail Solutions, Ride Center, and Gateway Trail programs and managed the Subaru/IMBA Trail Care Crew and National Mountain Bike Patrol programs. He has facilitated collaboration between dozens of mountain bike groups and land management agencies, including engagements with Cleveland, Lake, and Summit Metro Parks and numerous OH State Parks.

Linnenburger received Bachelor's degrees in Biology and Secondary Education from Eckerd College in St. Petersburg, FL and a Masters of Environmental Management degree at Duke University's Nicholas School with a focus on wetland/stream ecology, restoration, and habitat planning.



KAY-LINN

### DIVISION OF SCOPE

- Support, Trail Assessment, Signage/Wayfinding
- Lead, Risk Assessment, Stakeholder Outreach, Redevelopment Plan







### PROFESSIONAL EXPERIENCE

**Principal**

*Kay-Linn Enterprises, LLC*

**2009-present**

**Director of Field Programs**

*International Mountain Bicycling Association, Boulder, CO*

**2006-2008**

Directed the development and implementation of nationwide education (Subaru/IMBA Trail Care Crew), stewardship (National Mountain Bike Patrol), and trail contracting (IMBA Trail Solutions) and model trail initiative (Ride Centers, Gateway Trails) programs. Managed the relationship between IMBA and 650 affiliated grassroots mountain bike clubs. Oversaw 10 full-time staff. Grew and managed a budget from \$800,000 to \$1.6million through collaborative partnership development, foundation and recreational trails program grants, and federal appropriation requests. Served as the IMBA liaison to the Federal Highways Administration's Recreational Trails Program, American Trails, National Trails Training Partnership, and Professional Trailbuilders Association. Regularly presented trail development and management seminars at national/regional conferences and upon request to federal land managing agencies, state trail coordinators, and outdoor industry members.

**Trail Solutions Manager**

*International Mountain Bicycling Association, Boulder, CO*

**2004-2006**

Developed and managed Trail Solutions, a successful nationwide, fee-based trail contracting program. Grew the program from 2 to 5 full-time staff and tripled the revenue generation with a minimal capital investment. Services included trail assessment, planning, design, and construction for clients representing federal, state, and local land management agencies, ski areas, land trusts, friends' groups, and bicycle companies.

**Subaru/IMBA Trail Care Crew**

*International Mountain Bicycling Association, Nationwide*

**2002-2004**

Traveled full-time and co-taught 150 weekend-long sustainable trailbuilding seminars for volunteers and land managers in 32 states. Curricula strove to develop each community's physical infrastructure and volunteer capacity for trails that would highlight rather than erode natural resources. Played a major development role in IMBA's widely acclaimed trailbuilding and management texts.

**Project Manager/Environmental Scientist**

*Spangler Environmental Inc., Raleigh and Wilmington, NC and Barrett Kays and Assoc., Raleigh, NC*

**1998-2002**

Oversaw multi-disciplinary environmental planning projects revolving around regulatory enforcement (mostly North Carolina Department of Environment and Natural Resources and US Environmental Protection Agency) and private residential and commercial development for a start-up environmental services firm in Raleigh, NC. Projects were located in North Carolina, South Carolina, and Virginia and included environmentally sensitive site planning, endangered species assessment, wetland/stream delineation, mitigation planning, and wetland/stream restoration. Developed a client and project base that necessitated a second office for the firm in Wilmington, NC and managed the development of that office.



### Science Educator

1994-1996

*Gibbs and Thom Howard High Schools, St. Petersburg, FL*

Taught marine biology, environmental science, and biology curricula in urban schools, focusing on bringing students toward a better understanding of natural processes and the role of humans in impacting and ultimately rebuilding the natural environment.

## EDUCATION

### Master's of Environmental Management

1996-1998

*Nicholas School of the Environment, Duke University, Durham, NC*

Concentration: Water Resource Management, with a focus on stream and wetland ecology and restoration.

Master's Project: Habitat Conservation Plan for the Upper Tar River, North Carolina (for the North Carolina Nature Conservancy)

Course of Study: 58 credits in two-year program with courses in wetland ecology, wetland management, wetland hydrology, ecological restoration, stream geomorphology, wetland flora of North Carolina, sediment transport, environmental toxicology, fate of organic contaminants in the aquatic environment, ecosystem management, water quality management, environmental planning, resource and environmental policy, and environmental decision analysis.

### Bachelor of Arts

1990-1994

*Eckerd College, St. Petersburg, FL*

Majors: Biology and Secondary Education, Presidential Scholar

Course of Study: Completed a diverse natural sciences curriculum, intensive one-year secondary education certification program, liberal arts enrichment courses as part of honor's program, studied German history/cultural development and desert ecology off-campus during month-long winter terms, and played for and captained NCAA soccer team.

## PROFESSIONAL ACTIVITIES

### Board Member

*American Trails*

Served on the Board of American Trails (2006-2017), a nationwide non-profit that is pursuing a national infrastructure of trails and greenways and serves a full range of activities through education, partnerships, and timely information resources. Assisted with the development of the 2008 - 2014 National Trail Symposia and currently serves as the Chair of the Development Committee.

### Mountain Bike Liaison

*National Trails Training Partnership*

Currently working with this public-private working group made up of representatives from the federal land managing agencies and trail-based special interest groups. The Partnership strives to promote and develop trail education outlets for volunteers and agency personnel to increase the country's capacity for trails and the corresponding natural resource and health benefits.

### Former Board Member

*Professional Trailbuilders Association*

Played an integral role in broadening the membership of the Professional Trailbuilders Association, which has increased by 30% in the last four years. Encouraged the active involvement of new members, provided business development and management mentoring for new trail contractors, and on-the-job training with established contractors on large projects. Served as Committee Chair for the development of PTBA's first Strategic Plan.





WITH ASSISTANCE FROM:



## PART II: COST PROPOSAL



## CONTACT INFORMATION

This Proposal is Offered By:

Dr. Jeremy Wimpey

Applied Trails Research

1310 N. Allen St.

State College, PA 16803

443.629.2630

[jeremyw@appliedtrailsresearch.com](mailto:jeremyw@appliedtrailsresearch.com)



**Clinton-Huron Metropolitan Authority  
Sheldon and Trolley Natural Surface Trails Redvelopment Plan  
Stony Creek MetroPark**

Page 1 1/3/2018



## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Nina Kelly, Manager of Planning  
 Subject: Report – Lake St. Clair Beach Restoration  
 Date: June 7, 2018

### Action Requested: Motion to Receive and File

That the Board of Commissioners' receive and file the Lake St. Clair Beach Restoration report as recommended by Manager of Planning Nina Kelly and staff.

**Background:** Planning, Natural Resources, Engineering, and Operations staff have been meeting since March 2018 with partners in Macomb County to discuss opportunities for improving water quality at Lake St. Clair Metropark and elsewhere within the Lake St. Clair watershed area. The impetus for beginning these discussions was the discovery of a significant grant funding opportunity through the Environmental Protection Agency (EPA) and Great Lakes Restoration Initiative (GLRI) based on a previously-prepared conceptual design for eliminating contamination sources resulting in beach closures.

The original conceptual design, prepared in 2014 by Environmental Consulting & Technology, Inc. (ECT), called for shoreline naturalization, turf reduction and the construction of dunes along the existing beach. During conversations held thus far in 2018, Metroparks staff have advocated strongly to include three key components into the planning process for any future restoration initiatives at Lake St. Clair Metropark: (1) a comprehensive review of all previous water quality-related studies and assessments on Lake St. Clair with a summary of key findings, (2) facilitation of a robust public engagement process during the design phase, resulting in (3) multiple conceptual designs for evaluation by staff and the Board, which take into account the existing recreational uses of the beach area.

ECT has submitted a proposal to complete the review of previous studies, develop an initial cost estimate based on anticipated best management practices to be implemented during the project, and finally to develop an application for the available grant funding.

A preliminary timeline is as follows:

<u>Task</u>	<u>Complete By</u>
• Complete review of studies	June 2018
• Submit grant application	August 2018
• Informational public meeting <ul style="list-style-type: none"> <li>◦ <i>Present summary of findings from review of studies, outline of project timeline and public input opportunities</i></li> </ul>	Sept 2018
• Design and public engagement	May 2019
• Construction	Fall 2019



## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
From: Ryan Colliton, Manager of Natural Resources and Environmental Compliance  
Subject: Report – Mowing Plan  
Date: June 7, 2018

### **Action Requested: Motion to Receive and File**

That the Board of Commissioners' receive and file this report as recommended by Manager of Natural Resources and Regulatory Compliance and staff.

**Fiscal Impact:** Anticipating a positive fiscal impact resulting from lower fuel consumption and less equipment wear.

**Background:** In 2015, the Metroparks developed a comprehensive mowing plan to address issues with invasive species, recreation and aesthetics. This plan was created with the explicit goal of revisiting the plan every two years to address any corrections or changing realities in the Metroparks.

In the Fall 2017, Natural Resources staff met with park operations staff including management and grounds maintenance staff to address needed updates to the plan. At the time, several needs were identified based on the results of executing the 2015 plan.

The first was to address the goals of annual mow areas, which were previously no mow areas and the definition of annual mow was changed to grassland management. This definition allows staff to address the invasive species that may be present in the grassland areas directly without the need to mow the entire area. This is viewed as a reasonable compromise between historic no mow areas that became breeding grounds for invasive species and annual mow areas which were so extensive park resources were strained to complete the task.

Second, the Natural Resources department is taking an active role communicating with the parks regarding critical times of the year such as spring nesting season for grassland birds, Eastern Massasauga Rattlesnake habitat and other wildlife.

Third, staff is scheduling site visits with all the parks to view implementation on the ground and discuss any questions regarding the plan. It is the Natural Resources department goal that these changes will allow the Metroparks to maintain recreational areas and enhance conservation efforts system wide.



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Mike Henkel, Manager of Engineering  
 Project No: 516-18-482  
 Project Title: Bids – Maintenance Aboveground Storage Tank (AST) Installation  
 Project Type: Capital Improvement  
 Location: Huron Meadows Metropark  
 Date: June 7, 2018

Bids Received: May 24, 2018

## Action Requested: Motion to Approve

That the Board of Commissioners' (1) award Contract No. 516-18-482 to the low responsive, responsible bidder, Phoenix Environmental, Inc. in the amount of \$122,748.22; and (2) approve the transfer of \$122,748.22 from Fund balance to cover the cost of the project as recommended by Manager of Engineering Mike Henkel and staff.

**Fiscal Impact:** This project was not budgeted in 2018; however, the project was planned to be completed in 2022. The project cost would shift the planned expense from 2022 to the current year.

**Scope of Work:** The work is to include furnishing all labor, equipment and materials necessary for the installation of an above ground storage tank for 1,000 gallons of unleaded and 1,000 gallons of diesel fuel, associated controls, dispensers, concrete pad, path, site work and restoration.

**Background:** The underground fuel storage tanks that supply the maintenance facility at Huron Meadows were removed in April 2018, which resulted in a clean close for the site.

The decision to remove the tanks began when issues arose with the tanks leak detection system. Staff tried to obtain replacement parts; however, the existing probe system was antiquated and parts were no longer available. Knowing that the tanks were planned to be removed in 2022 and that any cost to make improvements would be for a short-time period, staff made the decision to remove the tanks and proceed with the installation of the new system.

The two 1,000-gallon underground storage tanks were installed in 1984; the park is currently operating on temporary tanks. After installation the tanks will be connected to the computerized fuel management system. Once this project is complete there will be five sites left with underground storage tanks of the original 19 sites.

<u>Contractor</u>	<u>City</u>	<u>Amount</u>
Phoenix Environmental	Plymouth	\$122,748.42
Oscar W. Larson Company	Clarkston	\$126,990.00
R.W. Mercer Co.	Redford	\$211,262.00
Budget Amount for Contract Services and Administration		\$ 0.00
Work Order Amount		
Contract Amount- Phoenix Environmental		\$122,748.42
Contract Administration		\$ 4,000.00
Total Proposed Work Order Amount(Rounded)		\$126,748.00

*This project was reported and publicly advertised in the following construction reporting outlets: MITN, Construction Association of Michigan, Reed Construction Data, Construction News Corporation, Construction News Service, HCMA Website, Builders Exchange of Michigan, McGraw Hill Dodge, Builders Exchange of Lansing and Central Michigan.*



# HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
 From: Mike Henkel, Manager of Engineering  
 Subject: Professional Design Services  
 Project Title: Approval – 26 Mile Road Bridge Rehabilitation and Pedestrian Bridge Design  
 Location: Stony Creek Metropark  
 Date: June 7, 2018

Proposal Received: May 22, 2018

## Action Requested: Motion to Approve

That the Board of Commissioners' (1) approve the proposal from IBI Group in the amount of \$104,500; and (2) authorize the transfer of \$78,000 from the Engineering Outside Services account to cover the cost of the project as recommended by Manager of Engineering Mike Henkel and staff.

**Fiscal Impact:** In the 2018 budget, \$26,000 was identified for design services to rehabilitate the 26 Mile Road Bridge. Additional funding in the amount of \$78,500 is available in the General Engineering Outside Services account to cover the cost of the design effort. Depending on the resulting design a reduced design effort may be possible.

**Scope of work:** The consultant will review existing conditions and record drawings, perform and assess the existing bridge and complete a geotechnical investigation. The work includes providing three design concept drawings, which will include widening the shared use path with a bridge deck extension and without. In addition, the consultant will provide details and cost estimates for bridge deck rehabilitation, traffic control plan, permits and the development of a complete set of construction documents.

**Background:** Staff chose the IBI Group proposal on their understanding, previous experience, identification of the project scope and the deliverables. The focus of the design is to rehabilitate the existing bridge deck including, joints, approaches, curb, structural spalls and railings. The services will also include the options for widening the bridge to allow for a wider shared use path, which is currently inadequate.

The existing path is approximately 6.5-feet wide. It may be possible to widen the existing shared use path without an extension to the bridge by eliminating the center concrete divider, which would result in a reduced design effort if possible. The existing concrete bridge was constructed in 1963, is approximately 63-feet wide and 125-feet long. In 1978, the outbound lane was modified to facilitate the walkway. In 1987 fencing was added to further separate the walkway.

<u>Consultant</u>	<u>City</u>	<u>Amount</u>
Anderson, Eckstein, and Westrick, Inc.	Shelby	\$ 46,000.00
DLZ Michigan, Inc.	Lansing	\$ 102,400.00
IBI Group	Southfield	\$ 104,500.00
Wade Trim	Taylor	\$ 185,270.00

**Attachment: IBI Group Proposal**





# IBI Group Proposal

26 Mile Road Bridge Rehabilitation And  
Pedestrian Bridge Design  
**Stony Creek Metropark**

IBI Group - 25200 Telegraph Rd - Southfield, Michigan 48033- 1.248.936.800 - [www.ibigroup.com](http://www.ibigroup.com)







**IBI GROUP MICHIGAN, LLC**  
25200 Telegraph Road – Suite 300  
Southfield MI 48033 USA  
tel 248 936 8000 fax 248 936 8111  
[ibigroup.com](http://ibigroup.com)

May 22, 2018

Mr. Jason Kulongowski  
Huron-Clinton Metropolitan Authority  
Engineering Department  
13000 High Ridge Drive  
Brighton, Michigan 48114

**RE: P-509-18-550 - 26 MILE ROAD BRIDGE REHABILITATION AND PEDESTRIAN BRIDGE DESIGN**

Dear Mr. Kulongowski:

Thank you for the opportunity to submit our qualifications for the 26 Mile Road Bridge Rehabilitation and Pedestrian Bridge design project. IBI Group (IBI) assists clients with planning, design, and implementation of public and private infrastructure and building projects. We specialize in providing comprehensive management and design services from conception through completion.

IBI Group takes a **TOTAL PROJECT APPROACH** to assignments we are awarded. This approach focuses on elements affecting the critical path of the project, such as environmental permitting and governmental coordination, and utility issues. We have consistently found that these areas can dramatically impact the schedule and budget of most projects; therefore we have developed additional core competencies in these areas in addition to our traditional project skills.

Recent related projects include: Evergreen Road over I-96 & CSX Railroad, Wayne County, MI; Oasis Trail & Lunken-Armleder Trail and Bridge in Hamilton County, OH; Little Miami Scenic Trail - Beechmont Avenue Bridge in Cincinnati, OH.

Our innovative management philosophy has allowed our firm to lead and excel in many projects. For example, the Cathedral Avenue Walkover over M-39 in Wayne County, MI earned the *MDOT / ACEC 2017 Partnership Charter Award*. This project is an excellent example of IBI Group's project management expertise and clearly illustrate our ability to implement an innovative approach throughout the design, utility coordination, and construction phases of a project.

**Why IBI's TOTAL PROJECT APPROACH Works?**

**It Saves Time** - Having the ability to provide all project development services in-house allows us to progress smoothly from one task to another and therefore maintain control of the project schedule. Often, we are able to complete multiple tasks concurrently to bring projects to completion ahead of schedule.

**It Saves Money** - Cost savings result from lack of multiple consultant coordination, and efficient use of our resources. For example, we can assign our junior and senior staff to project tasks according to the level of expertise needed, so clients get the most value for their dollar without compromising quality. Time saved also translates into additional cost savings.

**It Improves Quality** – IBI Group's traditional and value-added services, cross-trained personnel, and state-of-the-art equipment combine to achieve high-quality project results. Through stringent quality assurance and control measures, we ensure the contractor has clear and detailed plans, which in turn minimizes change orders.

As a resident of the Metro Detroit area, I take particular pride in the work we do with the state and local agencies. I will serve as the primary client contact for HCMA. During my 33 years of experience, I have had the privilege to serve on dozens of local agency and MDOT projects, and I am excited to lead the IBI Team.

Please note that we believe that construction staking is best included in the project as one of the contractor responsibilities. Also, construction oversight is best defined after the contractor is selected and the contractor presents the construction schedule. Only then we can define the approximate required level of efforts.

We look forward to working with you on upcoming project. If you have questions or need additional information, please do not hesitate to contact me on my cell phone at 248-982-8581

Sincerely,

Nazeeh Oudeh, PE

*Nazeeh Oudeh, PE* May 21 2018 5:29 PM

cosign

Nazeeh Oudeh, PE

Michigan Transportation Department Manager

Cell: 248-982-8581

Nazeeh.oudeh@ibigroup.com

IBI Group is a globally integrated architectural, planning, engineering and technology firm. Since 1974 the firm has provided professional services for a wide range of projects across five continents. IBI Group professionals exercise a broad and sophisticated knowledge base that spans urban design and planning, building and landscape architecture, engineering, advanced transportation management and traffic systems, real estate analysis, communications specializations, and software development.

IBI Group has over 40 years of experience with numerous repeat clients in the private and public sector including local, regional, and national government agencies. We are committed to environmentally responsible design and have the expertise needed to adhere to, and audit systems for, all appropriate compliance, codes, and requirements wherever we are working.

IBI Group's offices are global "virtual studios" that use a variety of collaborative technologies including video conferencing, intranet portals, and computer networks, enabling efficient team-wide communication across the globe. This system enables the firm to commit the best staff to each project and to use all resources and available expertise around the clock and around the world.



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2,500 DIFFERENCE-MAKERS  
AND COUNTING

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## QUICK FACTS

IBI Group  
Founded in 1974

2,500 employees

60 offices world-wide

IBI Group Ranks 6th in World  
Architecture's Top 100 Firms

\$355 Million in Fees in 2017

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Infrastructure

## MARKET SECTORS

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Aviation

Bus Rapid Transit Commercial

Educational

Energy

Events

Government

Healthcare

Hospitality

Industrial

Justice

Leisure/Theme Parks Logistics/

Supply Chain Manufacturing

Municipal

Science & Technology

Transit-Orientated Development

Transportation

## SERVICES

Architecture

Contract Administration

Engineering

Interiors

Landscape Architecture

Planning

Programming

Project/Program Management

Systems

Transportation



## ARCHITECTURE

- Programming
- Sophisticated Building Animations/Fly Arouns
- Master & Strategic Planning
- Construction and Contract Management
- Building Evaluations
- Smoke and Timed Egress Studies
- Code & Energy Studies
- Building Envelope Analysis
- Historic Rehabilitation & Preservation
- Field Representation
- Schematic & Conceptual Studies
- Cost Estimating
- 3D Drawing & Details
- Signage
- Interior Design

## CIVIL ENGINEERING

- Test Tracks
- Utility Design
- Wastewater Treatment
- Tank Farms
- Pavement Design
- Drainage
- Railroad
- Surveying
- Truck Docks: Retaining walls
- Truck Scales
- Vehicle Turning Studies
- Site Development
- Site Selection
- Planning
- Due Diligence
- Site Plan & Zoning approvals
- Site Clearing and demolition
- Site grading
- Earthwork calculations (cut/ fill, site balance)
- Erosion & Sedimentation Control
- Retaining walls / Sheet piling

## STRUCTURAL ENGINEERING

- Full-service Structural Engineering for Buildings and Special Structures
- Reinforced Concrete
- Pre-stressed Concrete
- Structural Steel and Cold-Formed Steel
- Structural Masonry and Structural Wood
- Static/Dynamic Analysis and Design of Equipment Foundations
- Non-Structural and Structural Slabs-On-Ground
- Noise and Vibration Analysis and Design
- Structural Condition Assessment
- Structural Retro fit
- Structural Rehabilitation
- Seismic and Wind Analysis/Evaluation
- Building Code Analysis
- Construction Engineering
- Special Inspections
- Forensic Structural Engineering
- Peer Review and Value Engineering
- Digital Surveying and Modeling
- Structural Building Information Modeling

## MECHANICAL ENGINEERING

- Thermal Storage
- Energy Value Analysis/ Audits
- Emission (Engine) Controls
- Test Cells
- Computer Room
- Fire Protection
- Fuel Tank Farms
- Combined Heat & Power
- Site & Controls
- HVAC Design
- Industrial Ventilation
- Plumbing & Piping Design

## ELECTRICAL ENGINEERING

- Site Power Distribution
- Site Roadway & Parking Lot Lights
- Security Systems
- Fire Alarms/Code Compliance
- Design Network Infrastructure
- Short Circuit Study Calculations
- Interior and exterior lighting systems
- Power distribution

## INDUSTRIAL

- Process Planning and Selection
- Product Manufacturing Analysis
- Computer Integrated Manufacturing
- Robotics and Custom Machine Design
- Manufacturing Information Systems
- Equipment and System Procurement
- Mist and Dust Collection and Control
- Facility Planning
- 3-D Scanning and Modeling
- 3-D Facility and Process Design
- Equipment Installation and Fit-Up
- Move Management
- Fall Protection Design
- Lean Manufacturing





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	<b>B</b>	Understanding of Project and Tasks
	<b>C</b>	Management Summary, Work Plan, Schedule
	<b>D</b>	Personnel
	<b>E</b>	Budget Review
	<b>F</b>	References
<b>PART II</b>	<b>A</b>	Cost Proposal



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# Part - I

## Section - A

General Information and Project Team



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
Stony Creek Metropark

# General Information

**IBI Group** is pleased to submit a proposal for the 26 Mile Road Bridge Rehabilitation and Pedestrian Bridge Design project for the Huron-Clinton Metropolitan Authority (HCMA). Our firm has built a rich history of providing exceptional design engineering services, including numerous trail and shared-use path projects in the United States, Canada and internationally. This proposal provides details of our team and the seasoned engineers with their track record. Our key staff assigned to this project have a record of excellence on challenging MDOT projects that won ASCE and ACEC awards

IBI has assembled a team of professionals with deep bridge and road rehabilitation design and construction experience with local agencies and MDOT design. The team consists of IBI, SSI and GEI.

IBI has assembled a team of professionals with a breadth of bridge and road rehabilitation design and construction experience in service to local agencies and MDOT. The team consists of IBI Group Michigan, LLC (IBI), Surveying Solutions, Inc. (SSI), and GEI Consultants of Michigan, P.C. (GEI).

As a corporation licensed in the state of Michigan, **IBI** will be the prime consultant responsible for:

- Overall Project Management.
- Bridge Design.
- Roadway and Approach Design.
- Maintenance of Traffic Design.
- Traffic Signing (As Needed).
- Pavement Marking.
- Mobility Analysis / Traffic Management Plan.
- Oversight of subconsultants, SSI and GEI.



**Surveying Solutions, Inc. (SSI)** a Michigan Corporation will function as the survey subconsultant and will be responsible for the structure and road design surveys.

**GEI Consultants of Michigan, P.C. (GEI)** is a national consulting firm providing geotechnical, geo-environmental, water resources, and construction engineering services. Founded in 1970, GEI maintains a staff of over 850 dedicated professionals located in 38 offices throughout the United States and has completed over 35,000 projects worldwide. GEI has extensive experience in shallow and deep foundations, soil improvement and excavation support and slope stability design on projects across the country. Our team includes professional staff with years of experience in foundation evaluation, design and construction. These individuals have extensive experience with infrastructure projects across the country and overseas.

**The IBI Team** is prequalified with MDOT for Bridge Design, Road Design and Geometric Studies. We have successfully completed many projects of similar scope and have intimate knowledge of the process involved to achieve HCMA's goals. **IBI is proficient with AutoCAD Civil 3D**, and has recently completed the rehabilitation of the Evergreen Road bridge over I-96 & CSX Railroad, one of the "big ten" bridges in Michigan. The rehabilitation work for this 19 span MDOT structure included deep concrete overlay, transverse deck joint replacement, partial sidewalk replacement, railing and fencing repairs, railing retrofit at light poles, superstructure retrofit at proposed signal poles, and substructure repairs. Special construction sequencing details were developed for 2,700 linear feet of sidewalk replacement to remediate a potential safety condition in the adjacent existing deficient barrier construction.



## IBI Group Michigan LLC

25200 Telegraph Rd, Suite 300  
Southfield, MI 48033

**Federal ID Number - 26-0107755**

## Point Of Communications

Nazeeh Oudeh, PE  
Project Manager  
nazeeh.oudeh@ibigroup.com  
248-982-8581  
25200 Telegraph Road, Suit 300  
Southfield, Mi



## SSI

### Survey Sub-Consultant

Jeffrey Bartlett, PS  
4471 M-61  
Standish, MI 48658  
989-846-6601



## GEI

### Geotech Sub-Consultant

Micheal Gentner, PE  
990 Lalley Road  
Iron River, MI 49935  
906-214-4140



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ID Number: **B1879V**  
Name: **IBI GROUP MICHIGAN, LLC**  
Payment Confirmation Nbr: **17092615220758**  
Filed By: **KATHLEEN A. LANGE, SPECIAL MANAGER**  
Title: **AUTHORIZED AGENT**  
Phone: **561-694-8107**  
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# Part - I

## Section - B

Understanding of Project and Tasks



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
**Stony Creek Metropark**



# Understanding of Project and Tasks

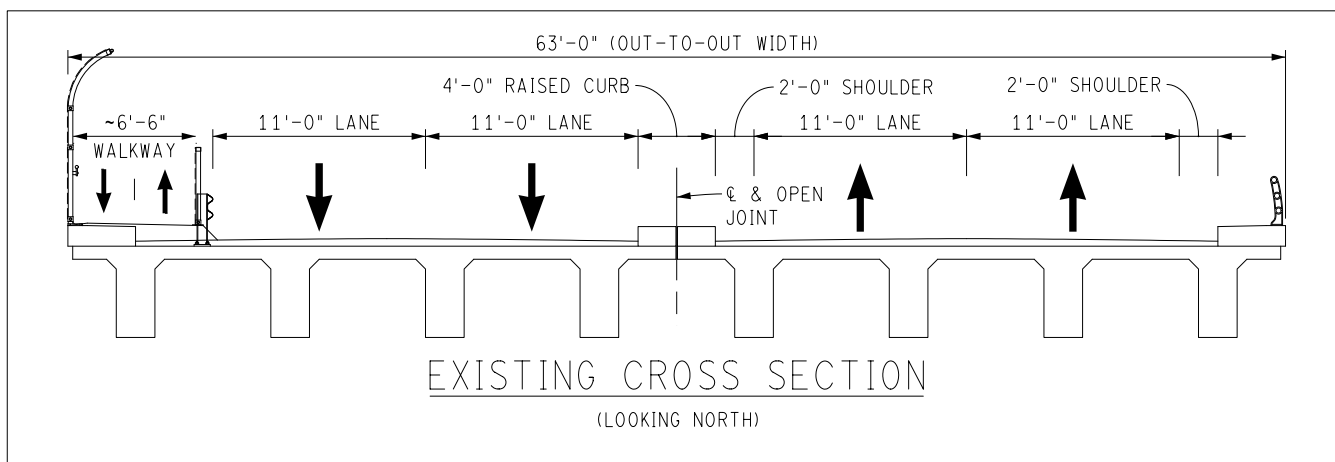
The IBI Team conducted a field review of the Stoney Creek Bridge over 26 Mile Road to assist in developing our understanding of the scope of work. During the visit, we identified project specific issues that may impact the project development.

The anticipated scope of rehabilitation work as stated in the HCMA Request for Proposal includes: bridge deck rehabilitation, deck joint replacement, approach work, curb replacement, substructure and superstructure concrete patching, railing replacement. Our team will work with HCMA to select the most economical and desirable option for the shared use path (SUP) crossing, and all rehabilitation design will be coordinated with the selected structure option. Additional rehabilitation work which will be investigated by the IBI Team may include: shallow or deep overlay of the deck, and adjustments to the alignment of the road and/or the SUP which may be needed.



## Existing Conditions

1. The existing 26 Mile Road vehicular bridge is at the Stony Creek park, a HCMA park. The existing structure (circa 1963) is an arch-type structure approximately 63' wide by 125' long. The original cross section has been modified over the years and currently appears as shown below:
2. The bridge approaches will require modifications to accommodate the proposed bridge overlay profile and new drainage conditions.
3. Guardrail transitions and anchorages to the bridge barrier are required.
4. Pedestrian fencing is substandard, upgrade will be as required by Macomb County/HCMA; otherwise a 54" railing may be utilized
5. A crossing over 26 Mile Road is needed for a shared use path, whether on the (modified) existing structure or on a new pedestrian bridge adjacent to the existing structure.

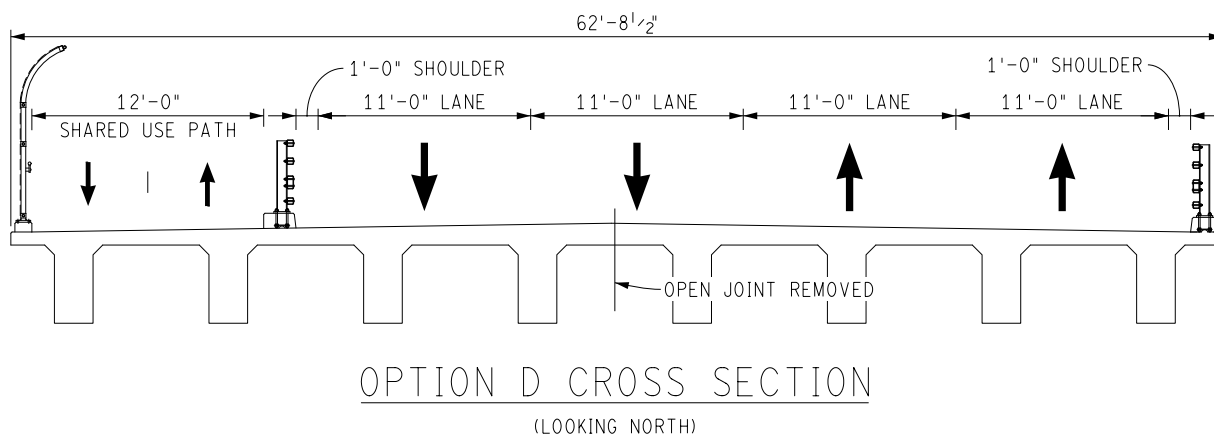


## The IBI Team understands that the RFP request is to provide the HCMA with the following scope of services:

1. Site investigation, available as-built plan reviews, utility contacts and location investigations.
2. A study that evaluates up to three possible feasible options (in addition to IBI's proposed option) to achieve the HCMA goals. The study will yield the selected design option.
3. A site survey that fits the selected design option requirement.
4. Design services of the selected option including drawings, estimate and technical specifications allowing the HCMA to bid and successfully construct it.

The options are:

- i. **Option A,**  
Existing bridge rehabilitation with a widening accommodating a shared use path. The widening will be accomplished by replacing the west wingwalls with new abutments for the shared use path and a prefabricated steel superstructure, or prefabricated prestressed concrete superstructure.
- ii. **Option B,**  
Existing bridge rehabilitation with a new bridge accommodating the shared use path. We anticipate a prefabricated steel bridge or precast beams on custom designed footings.
- iii. **Option C,**  
Existing bridge rehabilitation maintaining the existing geometry with a narrow shared use path, as allowed by the existing bridge deck width.
- iv. **Option D,**  
IBI proposed option to rehabilitate the existing and modifying the deck layout to accommodate a shared use path, thus fulfilling all of the HCMA goals without building a new pedestrian bridge or widening the existing structure. The center longitudinal joint and curb will be eliminated, lanes will be shifted to the east to make room for a 12' SUP. We understand that the path must be 10' wide with 2' wide clear zone per side; however, on bridges, 1' clear zone has been used by MDOT and proved adequate.



5. Design assistance during construction will be as indicated in the price proposal.

- **The anticipated scope of bridge rehabilitation work as stated in the HCMA RFP includes:**
  1. Remove the existing deck overlay, railings and brush blocks.
  2. Place a new concrete overlay with reinforcement tied to the existing rebars.
  3. Place new bridge railings, MDOT type 4 or open tube railings with proper heights for vehicles and pedestrians.
  4. Patch the substructure as needed.
  5. Replace the bridge approaches with reinforced concrete slabs.
  6. Restore expansion joints at the approach slabs.
  7. Evaluate the drainage within the bridge influence area and provide proper drainage design.
- **The SUP will be provided as discussed above.** For the selected option, IBI will provide the necessary design for one item i-iv discussed above.

Due to high costs and environmental impacts caused by widening the existing bridge or adding a new SUP bridge, we anticipate the HCMA will achieve its goals using the proposed IBI option (Item iv, option D above):

1. The provided 12' wide SUP meets AASHTO requirements and MDOT standards.
2. The proposed 11' wide lanes meet AASHTO lane width for local streets.
3. The removal of the median curbs allows for switching traffic flow on the exit lanes to provide more entrance capacity during high volume periods.
4. We anticipate savings to include \$300K for the prefabricated bridge, \$200k for the new substructure, reduced engineering costs, no geotechnical investigation, and no environmental impacts.

- **If the selected option includes adding a new SUP, IBI will review the proposed alignment.**

The Preliminary Scope drawing on page 37 of the RFP shows the proposed prefabricated pedestrian bridge as skewed relative to its supports. Traditionally, these prefabricated bridges require a no-skew condition. Therefore in order for Option A cross section, the widened portions of the substructure will need to be laid out perpendicularly from the bridge. This implies that there will need to be a “kink” in the abutment face from the existing abutment to the widened abutment. From a design perspective, this is certainly possible, but it is uncommon and may be deemed complicated.

Similar concerns can be observed in the plan view of Option B as shown on the same drawing, though the apparent solution differs. Since the prefabricated bridge is away from the vehicular structure, it does not need to be parallel. Revising the alignment of the SUP at the 26 Mile Road crossing to be perpendicular to 26 Mile Road will actually decrease the span and result in cost savings. If it is desirable to have the structures parallel then the substructure units will need to be constructed perpendicular to the alignment as mentioned above, but for Option B, they will be independent from the existing abutments.

During the study phase of this project our team will present the different alignments possible to HCMA to arrive at the most desirable solution.

- **The geotechnical investigation and geotechnical design**

Our base scope of work will be to complete a total of two (2) geotechnical borings. The borings will be placed at the approach slab elevation of the main entrance bridge (i.e. within the ROW of the park entrance road), one near each of the planned pedestrian bridge footings. Final locations will be determined in the field and based on existing utilities and other conflicts that may exist. The drilling and sampling procedures will follow the requirements of MDOT “Geotechnical Investigation and Analysis Requirements for Structures”.

We assume that our field crew will have free access to the area. We will coordinate our schedule with the HCMA of any work schedule. If borings are required to be completed on weekends or during night shifts, additional drillings costs will be incurred. By placing the borings within the park entrance road ROW minimal traffic control devices will be required and should limit interruption to traffic. We anticipate some traffic cones and warning signs for temporary traffic control (i.e. “shoulder work” or similar) without lane closures will be suffice.

The geotechnical borings will be extended to a maximum depth of 60 feet below existing grade (approximate elevation 880 feet). Within each boring, representative soil samples will be obtained at 2.5-foot depth intervals to a depth of 10 feet, and at 5-foot depth intervals thereafter to the termination depth of the borings. Soil sampling will be performed in accordance with ASTM D1556 and/or D1557, respectively, for cohesionless and cohesive soils. Water levels in the borings will be observed while sampling, drilling, and upon completion of the drilling.

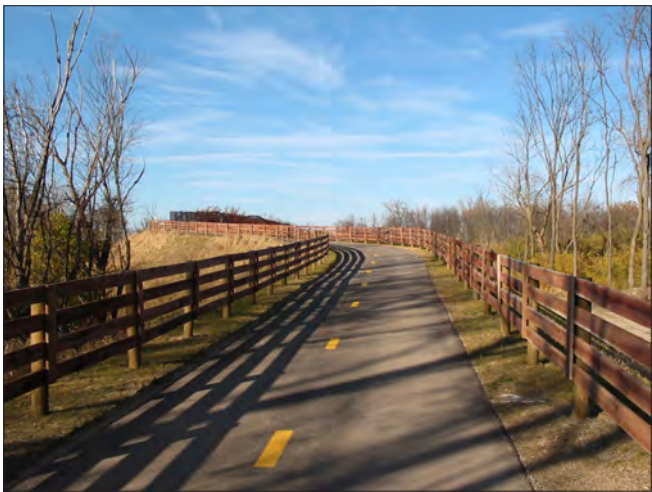
- **Site Survey**

The survey will fits the requirements of the selected design option. The project survey will include:

1. Survey of the bridge deck, approaches, SUP approaches and existing ground of the possible SUP bridge, and as required in the RFP.
2. Additional services if approved by the HCMA include full measurements of the existing bridge and road underneath, vertical and horizontal clearances and drainage ditches/culverts within the work limits.

- **Project requirements and summary of the plan for accomplishing the Project. (Please See Section C)**





Blacklick Creek Greenway Trail & Bridge  
Franklin County, OH

Areas of Practice

- Transportation Engineering
- Traffic
- Geomatics & Surveying
- Park & Trailway Design
- Right-of-Way Plan Development
- Maintenance of Traffic
- Bridge Design

Services

- Civil Engineering

**Location** Franklin County, OH  
**Size** Construction Cost \$2.1M  
**Status** Completed

**Key Personnel**

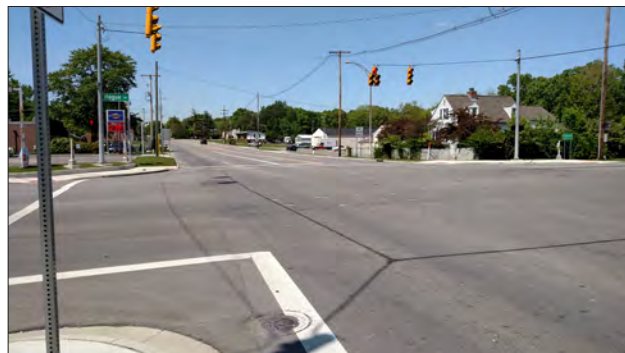
- Jeff Koehn, Project Executive
- Jeff Hackenbracht, Project Manager
- Jim Brenneman
- Ryan Hutson
- Dave Chiesa
- Jason Watt

**Reference**  
Steve Studenmund, Strategic Planning & Land Acquisition Manager  
Columbus & Franklin County Metro Parks  
Franklin County, OH

**T** 614 895 6231

IBI Group designed an eight-span pre-fabricated pedestrian bridge over U.S. 33 and Blacklick Creek and a one-mile trail segment. IBI Group also provided roadway/trail design; bridge engineering; FEMA hydraulics; signage and pavement markings; maintenance of traffic; drainage/storm sewer design at selected locations; erosion and sediment control; utility coordination; land surveying and cost estimates for the project. IBI Group coordinated all planning and design activities for this LPA project through ODOT District 6.

Quote from Steve Studenmund, Strategic Planning & Land Acquisition Manager for Columbus and Franklin County Metropolitan Park District: *“Having worked with qualified consultants throughout my career, (IBI Group)’s staff’s technical ability was only surpassed by their professionalism, patience, and persistence in helping us keep this project going through late changes requested by review agencies and utilities. Thanks to you and your staff..., this project is now ready for construction. The high-quality of plans provided by (IBI Group) and your bidding coordination and assistance, allowed ODOT and Metro Parks to select from eight qualified contractors with extremely comparable cost estimates.”*



## Fisher Road at Hague Avenue Improvements Columbus, OH

### Areas of Practice

- Transportation Engineering
- Traffic
- Geomatics & Surveying
- Maintenance of Traffic
- Lighting
- Public Involvement
- Stormwater Management
- Right-of-Way Plan Development
- Right-of-Way Acquisition
- Best Management Practices

### Services

- Civil Engineering

**Location** Columbus, OH

**Scope/Size** Scope/Size Of Project

**Status** Completed

### Key Personnel

- Jeff Hackenbracht, Project Executive
- Ryan Hutson, Role in Project
- Bob Wynd, Surveyor
- Dave Chiesa, Surveyor

### Reference

Cornell Robertson, PE, PS, Franklin County Engineer  
Franklin County Engineer  
Columbus, OH

**T** 614 525 3080

**F** 000 000 0000

**E** fracoeng@franklincountyengineer.org

The Franklin County Engineer selected IBI Group to prepare plans for widening Fisher Road including a new left turn lane in both directions at the intersection of Hague Avenue. Improvements included the reconstruction of roadway pavement and adjacent driveways, expanding the existing R/W, replacement of the existing drainage infrastructure, traffic signal replacement, and full replacement and widening of the bridge structure near the I-70 on-ramp. The single span replacement structure utilized composite prestressed concrete box beams on integral abutments. Additionally, the project included the addition of sanitary sewer and new waterline for the City of Columbus, as well as 5,000 feet of pavement planing and resurfacing along Hague Avenue. This project involved careful coordination with the City of Columbus and the Ohio Department of Transportation including an interchange modification study for the work in the vicinity of the I-70 on-ramp.



## HAM-75-(3.68)(3.85)(4.15) - Monmouth Street Bridge Improvements Hamilton County, OH

### Areas of Practice

- Bridge Engineering
- Traffic Design
- Traffic Signal Design

### Services

- Civil Engineering

**Location** Hamilton County, OH

**Size** Construction Cost \$7M

**Status** Completed

### Key Personnel

- Jeff Hackenbracht, Project Executive

### Reference

Tommy Arnold, P.E., Traffic Planning Engineer  
ODOT District 8

T 513 933 6588

This project consisted of reconstructing a new bridge for Monmouth Street over I-75, extending to Central Parkway on new alignment. In addition to bridge plans, IBI Group prepared roadway, lighting, drainage, maintenance of traffic, quantity and demolition plans. This bridge consists of a continuous curved steel plate girder with composite reinforced concrete stub abutments and “T” type piers on piling (span 128’ 6”, 160’ 6”, 128’ 6”). As a part of this project, a load rating analysis was performed.





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# Part - I

## Section - C

### Management Summary



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
**Stony Creek Metropark**

# Management Summary

## C. Management Summary,

- **Schedule:**

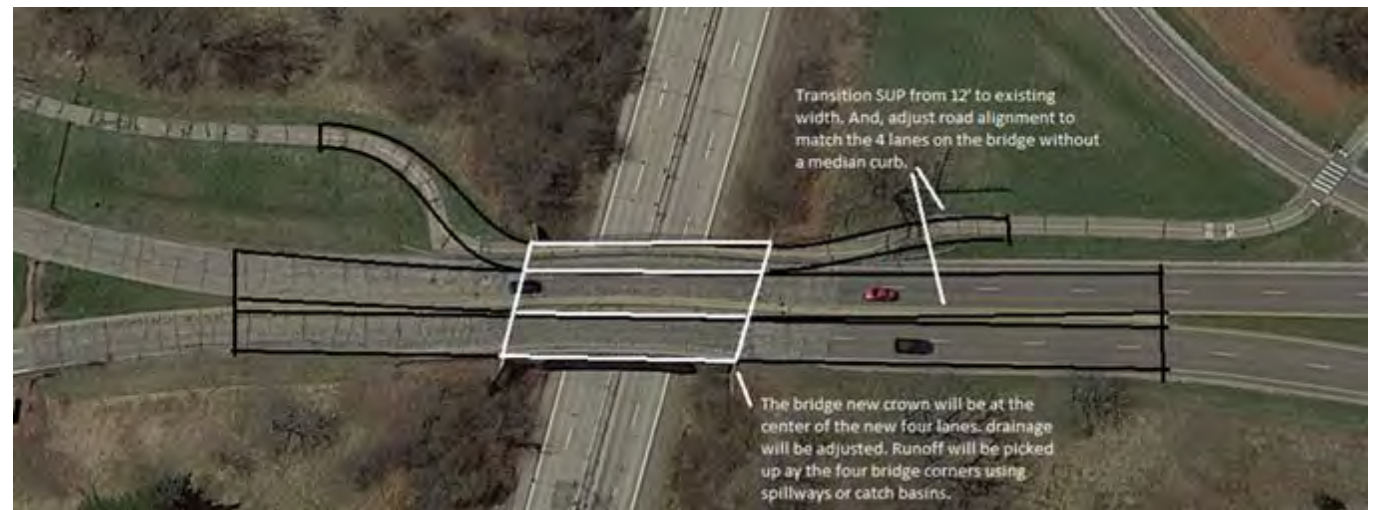
The project schedule in the RFP lists June 14, 2018 as the authorization date; and October 16, 2018 for the 100% drawings and technical specifications to HCMA. IBI is prepared to complete the tasks within the provided time. We are fully staffed and have the capacity to meet the project demands. To meet the RFP schedule, IBI will submit the 50% plans ahead of the required geotechnical investigation report. We will rely on the soil boring logs to determine the required type of footing.

Time critical tasks include survey, geotechnical work, utilities, and permits will be started immediately following the selection of the preferred option. The scopes of survey and geotechnical engineering will depend on the selected design option and the needed time to mobilize and complete the work, IBI is suggesting to alter the schedule as shown. The suggested schedule (subject to adjustments for Holidays and weekends) will allow for the contract award during the winter season without any delays in the construction schedule. Weather limitations will prevent work before March 30, 2019.

	Schedule PER RFP	Suggested Schedule	Comment
NTP	6/14/2018	6/14/2018	
Predesign Meeting	6/25/2018	6/25/2018	
Contact Utilities	6/26/2018	6/26/2018	
Submit Study/Design Options	7/17/2018	7/17/2018	
<b>Select Design Option</b>	<b>7/24/2018</b>	7/24/2018	
Receive Utilities information	7/24/2018	7/24/2018	
Submit Survey	8/24/2018	8/24/2018	need 30 days to schedule and complete
Conduct Geotechnical Investigation	8/24/2018	8/24/2018	need 30 days to schedule and complete
<b>Submit 50% plans</b>	<b>8/14/2018</b>	9/24/2018	30 days after survey and geotech
Review 50% Plans	8/21/2018	10/2/2018	
Submit Macomb County Permit	8/14/2018	10/9/2018	
<b>Submit 90% Plans</b>	<b>9/25/2018</b>	11/1/2018	30 Days after review
Review 90% plans	10/2/2018	11/15/2018	
Resolve Utility Conflicts	10/2/2018	11/15/2018	
<b>Submit 100% package</b>	<b>10/16/2018</b>	12/15/2018	
Bid opening	11/13/2018	1/29/2019	
Award	12/13/2018	2/28/2019	



- **Our Work Plan** will follow the scope of services and steps as defined in the RFP.



**Exhibit 1**

1. For options C and D, the road and bridge will be modified as shown in exhibit 1.
2. For all options, the new crown will be at the center of the proposed 4 lanes.
3. Drainage will be adjusted for runoff to run along the outside railings and then pick up by spillways or new catch basins.
4. The bridge center joint and median curbs will be removed.
5. The existing bridge deck will receive asphalt milling, hydrodemolishing and concrete overlay
6. Railings will be MDOT standard type 4 or tube railings.
7. The new pedestrian bridge will impact the 26 Mile Road drainage. IBI will design treatment for the impacted portions.



**Maintenance of Traffic (MOT)** will be

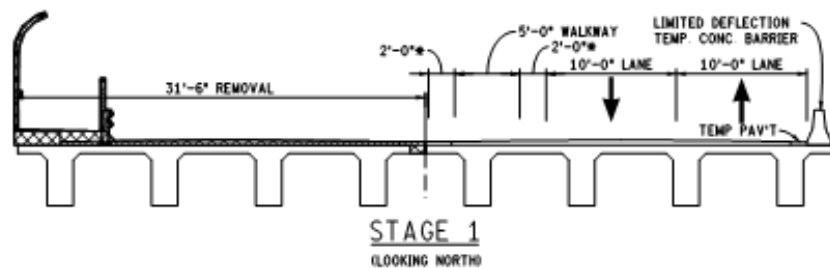
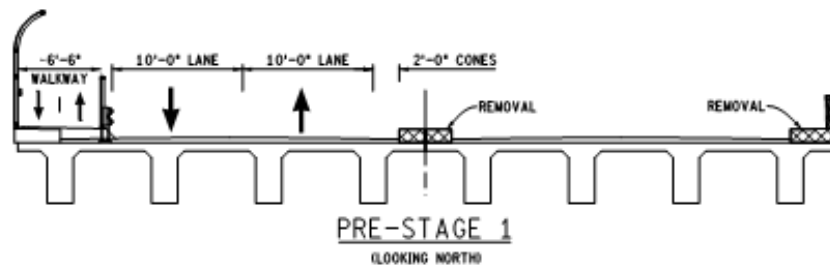
accomplished by using part-width construction in three stages. 5' wide pedestrian path will be maintained thru the construction zone. Bikes will have to be walked on the 5' wide walk. The MOT plan will follow the selected design option. The plan below is for options C and D. if option A or B is adopted, then the new pedestrian bridge can be accelerated and used for pedestrians while the vehicular bridge is rehabilitated.

One open lane for each direction will be maintained. 10' wide lanes will be adequate as no large truck will be anticipated. Each stage will require 2x10' lanes, 5' walk, 2x1' shy and 2x2' cones = 31' which is half the bridge width.

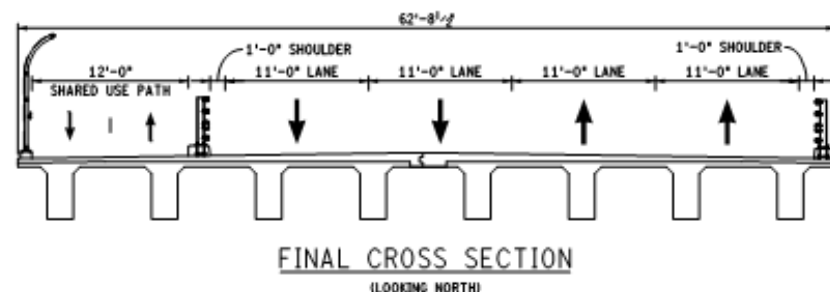
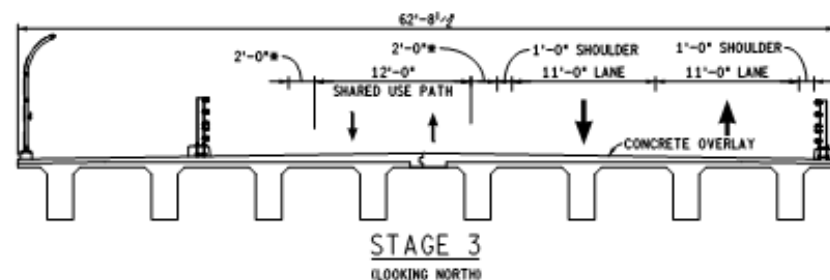
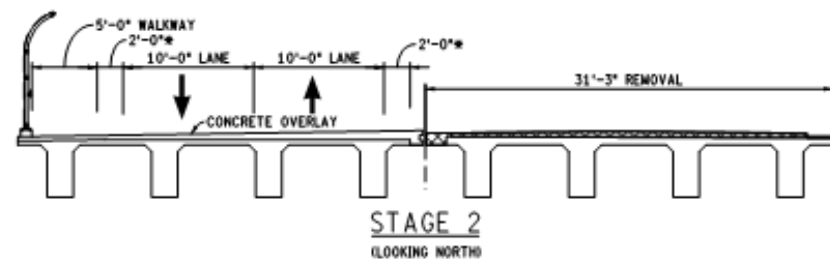
1. For stage one, traffic and pedestrian walk will be place at the east 31' of the bridge. Construction will be on the west 31'. The railing between the SUP and traffic will be installed after stage 2 to allow for vehicular use during stage two.

A sub-stage will be required to remove the median curbs from the bridge and approaches.

2. For stage two, traffic and pedestrian walk will be place at the west 31' of the bridge. Construction will be on the east 31'.
3. Stage three, traffic and pedestrians will be moved back onto the east half of the bridge to allow for the railing installation.
4. After the completion of the railing work, northbound traffic will be placed on the east lane, southbound traffic will be placed on the west lane. The center two lanes will then be used to construct the road median approaches and curbs.



\* = CONES OR TEMP. CONC. BARRIER



**Utilities on and under the bridge will be thoroughly investigated:**

8. For the known utilities on the deck, conduits will be designed in the new railings or deck to provide replacement for the existing tubes in the railings.
9. Impacted utilities on 26 Mile Road will be discussed and resolved with owners.



- **Design and construction cost controls are parts of our QC plan.** IBI will advise the HCMA of possible cost savings designs and/or potential increased costs due to unforeseen items.

1. Our proposed Option does save a considerable costs while allowing the HCMA to achieve its goals.
2. The savings can be used to mill and overlay the road on either side of the bridge.
3. As an added service, IBI will advise the HCMA of providing guardrails at the bridge approaches on 26 Mile Road to prevent an errant vehicle from hitting the bridge and to prevent high load hits when trucks use the shoulders. The arched shape of the bridge provides less than 14' clearance under the shoulders. See picture for scrapes indicating high load hits.

Damage from high load hits was repaired





## - QA/QC Plan and Constructability Review

**IBI is committed to providing professional/technical services and products that respond to client requirements.** As an ISO 9001:2008 certified company, this certification demonstrates our commitment to our quality management system and ensures a high level of quality assurance and quality control. To achieve this commitment to quality, IBI has implemented a Quality Management System (QMS) so that every staff member works toward a universal goal of client satisfaction. The QMS includes a commitment from top management to deliver the highest quality services. This commitment is embodied in our quality policy, supported by the hiring of highly qualified staff, and implemented on every job through the incorporation of our standard quality procedures. These procedures contribute to the delivery of quality service by defining and documenting processes so that all project activities are performed consistently.

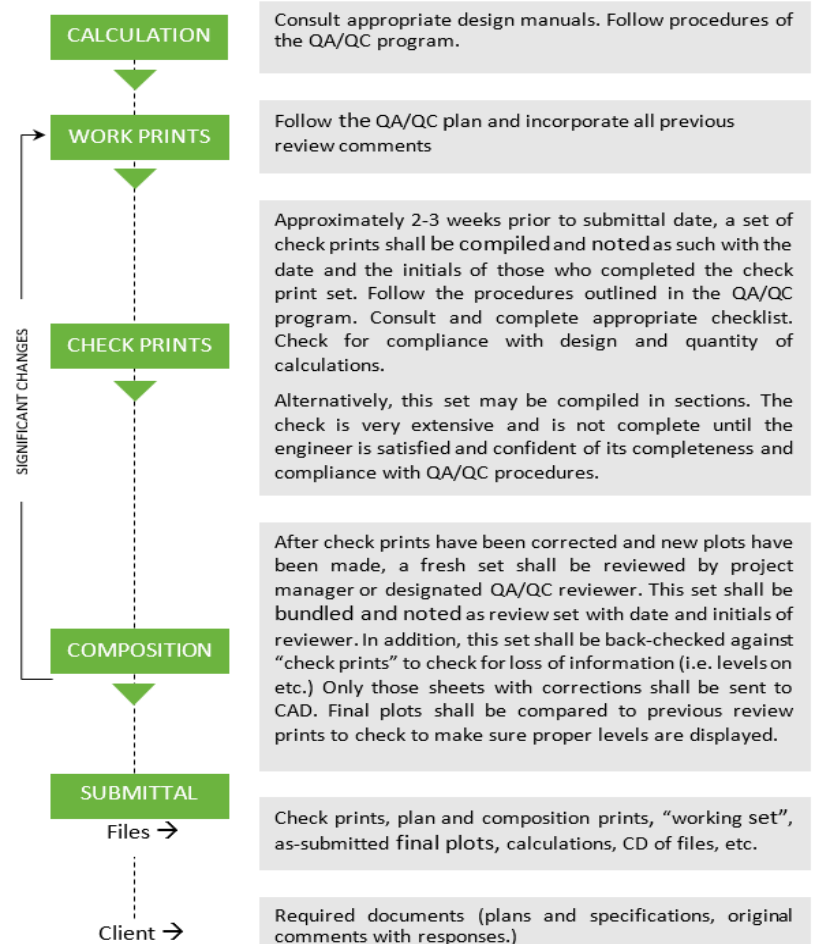
We emphasize state-of-the-art technical practices, coupled with efficient management to achieve a high level of service for our clients. At IBI, Quality Assurance/Quality Control operates under a distinct QA/QC procedure, based on our own and our clients' established guidelines.

**Our People:** IBI has high standards for employment qualifications, and jobs have been assigned in accordance with clearly demonstrated capabilities. Additionally, ongoing training ensures our project team personnel are up-to-date on the subject of the project, pertinent codes and standards, and on project QA/QC requirements.

**Our Structure:** IBI's Senior Planning and Engineering Managers are responsible for ensuring the planning and design team personnel execute projects in accordance with all procedural and client requirements. At the beginning of each project, procedures are established that provide step-by-step instructions on completing specific tasks. The overall objective is to record how the work was conducted to ensure data reliability.

**Our Procedures:** IBI uses a team-approach QA/QC process. In the first stage, the design team is responsible for accurate and efficient design. They review all of their work thoroughly as we prepare for a scheduled client submittal. Then, independent technical managers review the submittal to assure it has been thoroughly prepared and meets all technical standards. Finally, the QA/QC project team reviews the project while ensuring all items of the Scope of Services and industry standards are being met. This approach minimizes resubmittals and potential claims that may arise. All QA/QC documentation is retained in the project files through final construction and punch lists. It is available for review and can be copied and sent to the client for reference. IBI's QA/QC plan is designed to ensure quality performance and traceable results, and to ensure confidence in the documents prepared for all projects. The QA/QC Plan is comprehensive yet flexible, and can be modified to meet the specific needs of the project and client.

### QA/QC Process Flow Chart







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# Part - I

## Section - D & E

Personnel and Budget Review



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
Stony Creek Metropark

# Project Team



**Huron-Clinton Metropolitan Authority**  
Jason Kulongowski, PE

**Project Manager**  
Nazeeh Oudeh, PE\*

**Team Point Of Communications**  
248-982-8581  
25200 Telegraph Road, Suit 300  
Southfield, Mi  
FID# 26-0107755

IBI

**Qa/Qc & Constructability**  
Jeff Koehn, PE\*

IBI

**Road & Trail Design**

**Lead Road Design Engineer**  
Jefferey Glowka, PE\*

**Mobility**  
Brian Sedziol, PE\*

**Maintaing Traffic, Signing & Pavement Marking**  
Erin Wilkinson, EIT\*

**Estimates**  
Ran Ji, EIT

IBI

**Bridge Design**

**Lead Engineer**  
Steve Butler PE\*

**Support**  
Greg Jackman, PE \*  
  
Ran Ji, EIT

IBI

**Geotechnical Services**

**Lead Geotech Engineer**  
Micheal Gentner, PE\*

990 Lalley Road  
Iron River, MI 49935  
906-214-4140

GEI

**Survey**

**Lead Surveyor**  
Jeffrey Bartlett, PS\*

4471 M-61  
Standish, MI 48658  
989-846-6601

SSI

The Team Consists of IBI Group- MI, GEI, and SSI

\*Notes Key Staff

# Key Staff

The organization chart illustrates clear lines of communication amongst our team members who are summarized in Section A “General Information and Project Team” above. Additionally the resumes of key personnel have been included at the end of this proposal.

**Nazeeh Oudeh, PE** is **IBI’s Project Manager** and has strong experience with local agencies and MDOT road design projects. He will be the primary point-of-contact between HCMA, stakeholders and the IBI Team. Nazeeh has performed rural and urban road design such as:

- M-50 from US 127 to Napoleon Road, Jackson TSC;
- M-24 from Golden Gate to Drahner Road, Oakland TSC;
- I-94 BL from M-37 to I-94;
- M-59 from Opdyke Road to Crooks Road, Oakland TSC;
- Grand River Ave. in the Cities of Farmington and Farmington Hills; and
- City of Farmington Hills annual paving program.

Nazeeh has also managed complex projects such as the M-85 Boulevard and viaduct reconstruction in southwest Detroit, and the I-94 reconstruction in Dearborn, both located in Wayne County. Nazeeh prepared presentation boards and information packets for public meetings; he is comfortable with presentations and information meetings.

Nazeeh has over 32 years of diverse design and construction experience, including MDOT road and bridge design projects. He is adept at coordinating subconsultants and utilities. He will work closely with Jeff Glowka to evaluate the proposed design tasks and determine who will perform each task based on experience and capacity and design budget. He will ensure critical project risks are revealed and addressed.

**Jeff Koehn, PE (IBI)** will provide the **QA/QC & Constructability Review**. He will apply his 24 years of experience (including considerable bridge and trail projects) to verify proposed work is constructible, as shown on the study, and the intended design can be built according to practical and common construction practices. The review will ensure the order of construction is clear (where needed) and plans contain sufficient information to construct the job with minimal confusion and corresponding change orders.



### *Bridge Design Personnel*

**Steven Butler, PE (IBI)** will be the **lead bridge design engineer**; he will oversee Greg Jackman to develop bridge type and estimate for each alternative. He has over 15 years of civil engineering experience, with emphasis in structural bridge design. He has evaluated/developed superstructure and substructure plans for a variety of bridges within the states of Michigan, Ohio, and West Virginia. His experience extends to all phases of civil engineering projects, including preliminary studies and reports, site surveys and investigations, specifications, quantity take-offs, and cost estimates.

**Greg Jackman, PE (IBI)** has 7 years of experience and will be responsible for assisting in the design and analysis of the various bridge components, leading efforts in preparation of final project plans, preparing quantities, reviewing shop drawings and contributing to construction assistance.

### *Roadway Design Personnel*

**Jeffrey Glowka, PE (IBI)** will be the **lead road design engineer** and has significant experience with similar projects. He will lead design activities including preparation of concepts, typical cross-sections, cost estimates, and submittals. He has 19 years of diverse road experience on behalf of numerous agencies including design projects requiring coordination with several subconsultants and local agencies. Jeff is a resource for policies and procedures, and will ensure all HCMA requirements are met.

**Brian Sedziol, PE, PTOE (IBI)** will perform the mobility analysis and Transportation Management Plan (TMP) and has experience preparing roadway and traffic control plans and completing traffic studies. With 7 years of experience, he is well versed with horizontal and vertical alignments, profiles, and cross sections.

**Erin Wilkinson EIT (IBI)** will be in charge of traffic maintenance, signing, and pavement marking. Erin has 9 years of road design experience.

**Ran Ji EIT (IBI)** will complete the quantity takeoffs. Her attention to detail has enabled her to successfully estimate dozens of structures with hundreds of pay items.

### *Geotechnical Services Personnel*

**Michael C. Gentner, PE**, will be the GEI Project Manager, and primary contact with the Project Team. Mr. Gentner has over 24 years of geotechnical experience, and has directed hundreds of geotechnical explorations ranging from routine split-spoon and Shelby tube sampling to complex in-situ testing with vane shear, CPT, and pressuremeter methods. His engineering evaluation and design experience encompasses shallow and deep foundations, earth retention structures, pavements, and slope stability. Mr. Gentner will act as the licensed Profession Engineer for all submittals, will direct all technical staff, develop task order budgets, and be responsible for analysis and deliverables to the Project Team.

### *Survey Personnel*

**Jeffrey Bartlett, PS** will lead the project survey efforts on this project. He is well known throughout the industry as a leader in the field of survey. Jeffrey has completed hundreds of survey projects for MDOT. Examples include the I-75 Mega Project, The I-275 rehab from 5 Mile Road to I-696, M-94 in Marquette County and the M-46 in Saginaw County.



STAFF EDUCATION & EXPERIENCE REPORT		
NAME	TITLE	ROLE ON PROJECT
Nazeeh Oudeh, PE	Project Manager/Group Manager	Project Manager
COMPANY NAME	YEARS WITH IBI GROUP	TOTAL YEARS OF EXPERIENCE
IBI Group	1	31
EDUCATION		
B.S. in Civil Engineering, 1985, Wayne State University, Detroit, Michigan		
LICENSES / REGISTRATIONS		
Professional Engineer - MI # 37766 - 1992 <b>Training</b> includes: Road Safety Audits, Design of Non-motorized Facilities, ADA for Accessibility, Project Manager Boot Camp, Working with Historic Bridges, Maintenance of Asphalt Pavement, Concrete Overlays, Concrete Paving Inspection, Field Manager and Material Acceptance, Maintenance of Concrete Pavement.		
EXPERIENCE & QUALIFICATIONS		
Nazeeh's <b>design experience</b> includes reconstruction and rehabilitation of depressed and elevated, urban and rural freeways and associated bridges. He has combined road and bridge design experience. He proofed his ability to coordinate complex utility conflict resolutions and relocations, <b>coordinate public involvements and stakeholders</b> for Complex freeways, primary roads and bridges. He is known for innovative solutions to unique problems.		
SPECIFIC EXPERIENCE		
DATES	PROJECT I.D.	PROJECT DESCRIPTION AND ACTIVITY
2017 to ongoing	<b>American Center for Mobility</b> Washtenaw County, Michigan	<b>Project Manager</b> - This is a new <b>Design-Build, 2.5 miles long</b> autonomous vehicle test track simulating a limited access freeway and primary roads built on 80+ acres existing concrete slab from the old WW II bomber factory. <b>A full new interchange, road test loop, testing tunnel, three new bridges</b> , portions of the WB US-12 and two existing tri-level bridges were parts of the testing track. Included: the existing concrete slab is being used as the base for portions of the track creating the need for creative solutions managing thermal expansions effects, geometry studies, drainage management on large slabs with contaminated soils underneath, municipal utility relocations, tunnel construction within fill soils, multiple new roadways within limited ROW, and constructability issues due to tight schedule. Consultant Role: Prime Service Budget: \$250,000 Construction Budget: \$15,000,000 Client: Michigan Department of Transportation Client Contact: Mark Chaput, PE (517) 206-1802
2014 to 2014	<b>MDOT I-94 Freeway Scoping from Wyoming Avenue to I-96 Interchange</b> Wayne County, Michigan	<b>Project Manager</b> - This 6-lane divided depressed urban freeway included full <b>removal and replacement of the main line and ramp pavements, median barrier wall, and storm sewer system</b> . Recommendations were provided for shoulders, lighting, ITS, bridge filler wall rehabilitation, retaining wall repairs, signing, pavement markings, landscaping, <b>CSS, and public involvement</b> . Design items included soft soils and global stability issues, Storm sewer separation, low bridge underclearances, bridge struts within the pavement, and parabolic crown and alignment corrections; multiple design exceptions for narrow shoulders and design speeds for the freeway and crossing roads. Consultant Role: Prime Service Budget: \$244,000 Construction Budget: \$N/A Client: Michigan Department of Transportation Client Contact: Mr. Ashok Punjabi, PE (248) 483-5175
2014 to 2017	<b>US-127 from Washington Road to Begole Road</b> Gratiot County, Michigan	<b>Project Manager</b> - This 3R project rehabilitated <b>8.5 miles of freeway main line; all ramps at the weigh station; and Washington, Polk, Lincoln, and State Road interchanges</b> . It included concrete joint repairs, shoulder reconstruction, and HMA ramp and main line overlay. It included <b>bridge underclearance was improved by cutting road grades to meet the required 16 feet over the freeway. Full geometry evaluation/upgrades to correct crown and supers and provide transitions at bridge approaches</b> , full drainage study, MOT ATCs. Other items included geotechnical investigation, CPM, and constructability review were provided. Consultant Role: Prime Service Budget: \$1,399,458 Construction Budget: \$19,000,000 Client: Michigan Department of Transportation Client Contact: Jonathan Myers, PE (989) 775-6104

2012 to 2017	<b>MDOT Northbound US- 23 Ramp Extensions</b> Livingston and Washtenaw Counties, Michigan	<b>Project Manager - Ramps at M-36, 8 Mile Road, 6 Mile Road, and North Territorial Road were extended</b> to resolve congestion and rear-end crashes near Ann Arbor. Mitigation and Air Quality funds are used to extend acceleration lanes of four entrance ramps. The project includes <b>multiple public involvement meetings, safety studies and geometrics review, ROW evaluation</b> , utility coordination, drainage and runoff management plan, maintenance of traffic, pavement marking plans, permanent freeway signing, and construction critical path method. Later, <b>the project was expanded to become the first flex freeway in Michigan.</b> Consultant Role: Prime Service Budget: \$226,000 Construction Budget: \$6,000,000 Client: Michigan Department of Transportation Client Contact: Lynne Kirby, P.E. (810) 225-2627
2009 to 2010	<b>I-75 and 9 Mile Road Bridge</b> Oakland County, Michigan	<b>QA/QC Engineer</b> - After being destroyed by a tanker fire, MDOT used a design/build delivery method to procure this project to <b>replace the 9 Mile Bridge and reconstruct 0.5 miles of I-75.</b> Complex items included, the <b>I-75 was rebuilt in concrete and profile lowered to provide required under clearance for the bridges, an existing siphon under the John R Road pier south of 9 Mile Road was relocated, and a portion of the existing John R Road pier was reconstructed. The bridge included movable pier design.</b> Traffic engineering, signing, safety studies, and freeway lighting services were also part of the project. Consultant Role: Prime Service Budget: \$625,000 Construction Budget: \$12,500,000 Client: Michigan Department of Transportation Client Contact: Ms. Lori Swanson, P.E. (248) 451-0125
2005 to 2008	<b>M-102 (Eight Mile Road) M-1 (Woodward Avenue) Interchange</b> Wayne and Oakland Counties Ferndale and Detroit, Michigan	<b>Project Manager</b> - This historical <b>three-level urban interchange rehabilitation</b> project consisted of three bridges and approximately 0.5 miles of 6-lane pavement on M-102, 0.5 miles on M-1, and 5-lane pavement on service roads. <b>It included public involvement</b> , historic bridge railing restoration, complex MOT for the busiest intersection in Michigan, <b>crown modification</b> , historic lighting, pump station updates, private and public utilities including DWSD and PLD. Other items were curb and gutter, sidewalk, <b>vertical and horizontal alignment corrections</b> , drainage system, crash analysis, traffic signals designs, driveways, guardrail and attenuators, landscaping, <b>and design exceptions.</b> Consultant Role: Prime Service Budget: \$1,400,000 Construction Budget: \$16,000,000 Client: Michigan Department of Transportation Client Contact: Mark Sweeney P.E. (810) 225-2622
2009 to 2012	<b>M-85 (Fort Street) from Schaefer Highway to Oakwood</b> Wayne County Detroit, Michigan	<b>Project Manager</b> - This road reconstruction and 2200' long historic viaduct replacement project was approximately 1.12 miles of a multi-lane boulevard reconstruction and the replacement of the 3-bridge viaduct. The road included <b>pavement realignment and reconstruction</b> , storm runoff separation from CSO, <b>public meetings and shared CSS and shared path.</b> Also, it included several expensive private utility relocation and coordination, DWSD water main replacement, PLD lighting replacement, soft clay causing global stability and severe settlements leading to innovative structural solutions. Consultant Role: Prime Service Budget: \$3,950,000 Construction Budget: 44,000,000 Client: Michigan Department of Transportation Client Contact: Mark Sweeney P.E. (810) 225-2622
2003 to 2004	<b>I-94 from Rouge River to Wyoming Avenue</b> Dearborn, Wayne County Michigan	<b>Deputy Project Manager</b> - This Southeast Michigan ASCE award-winner, three miles, <b>4R depressed urban freeway reconstruction</b> project provided for <b>reconstruction of the freeway, four interchanges, 13 interchange ramps, and</b> the rehabilitation of 17 bridges. <b>Complex items were the curved ramp from US-12 to eastbound I-94</b> , global stability concerns, <b>railroad bridges with deficient vertical clearances</b> , and complex drainage issues. Major design considerations and innovations included (1) reconfiguration and replacement of all drainage with provisions for inline storage to handle current design requirements and to disconnect from CSO system, <b>(2) improved freeway and ramp geometry where project constraints permitted, and (3) improved underclearances to meet the design guidelines.</b> Consultant Role: Prime Service Budget: \$2,950,000 Construction Budget: \$45,000,000 Client: Michigan Department of Transportation Client Contact: Mark Sweeney P.E. (810) 225-2622

STAFF EDUCATION & EXPERIENCE REPORT		
NAME	TITLE	ROLE ON PROJECT
Jeffrey B. Koehn, PE, LEED AP, CPESC	Senior Principal	Constructability QC
COMPANY NAME	YEARS WITH IBI GROUP	TOTAL YEARS OF EXPERIENCE
IBI Group	21	24
EDUCATION		
B.S. Civil Engineering, Ohio State University, 1993		
LICENSES / REGISTRATIONS		
Professional Engineer, Michigan, #53009; Professional Engineer, Ohio, #63128; Pennsylvania, #050369-E; Kentucky, #21653; Tennessee, #107659; Indiana, #10200370; West Virginia, #16599; Connecticut, #25229; New York, #086480; North Carolina, #033533; Maryland, #36223; Illinois, #062061434; Kansas, #20995; Oklahoma, #24996; Florida, #72680; Massachusetts, #50977; LEED Accredited Professional; Certified Professional in Erosion and Sediment Control		
EXPERIENCE & QUALIFICATIONS		
Mr. Koehn's background in the study, management, design, funding and construction of infrastructure projects enables him to effectively supervise operations, production, and quality. As a firm principal, he is directly responsible for IBI's Global Transportation Services and he is able to dedicate any of IBI's resources to ensure exceptional project results. He has led large and innovative projects in both the public and private sectors. Mr. Koehn's daily responsibilities include management, design, and construction management/administration of transportation improvement projects, including: road widenings, new alignments, intersection design and access management; commercial, residential, industrial and institutional site developments; storm and sanitary sewer design; water systems; sanitary sewer; bridge and retaining wall analysis, inspection, evaluation, and design; multi-use trails; traffic control and signal plans; and environmental permitting.		
SPECIFIC EXPERIENCE		
DATES	PROJECT I.D.	PROJECT DESCRIPTION AND ACTIVITY
2008 to 2012	<b>SR 4 Bypass Avenue</b> Butler County Transportation Improvement District Butler County, Ohio	<b>QA/QC Lead Engineer-Design of over 4 miles of improvements to an existing two lane highway to become the first "Super Street" intersection corridor in Ohio.</b> The project included full reconstruction and widening; replacement of curbs, storm sewers, gutters, new water line, widening of two existing bridge structures over SR 129 and the Northfolk Southern RR; modifications and upgrades to existing traffic signals; traffic control plans; provisions for maintenance of traffic, and water quality. IBI also provided right-of-way plans and public involvement services (\$22 million est. construction cost). <b>Complex issues included: geometric studies for the conversion of a two-lane state road into a four lane divided facility in restrictive site conditions; development of Super Street intersection at three locations along the corridor;</b> development of special signage and traffic signal structures to accommodate the large intersections, development of individual signal timing for each intersection and coordination with the loon turn a rounds, coordination of a global maintenance of traffic scheme and significant utility coordination and relocation activities. Consultant Role: Prime      Service Cost: \$2,000,000      Construction Cost: \$21,000,000 Client: Butler County, TID      Client Contact: Ron Porter (513) 641-9077
2012 to 2014	<b>Union Airpark Boulevard</b> City of Union, Montgomery County, Ohio	<b>Project Manager -</b> Assisted the City of Union and the Montgomery County Transportation Improvement District with the construction of approximately 1.75 miles of new roadway located between SR 40 and Old Springfield Road. The improvements included proposed roadway with curb and gutter, storm sewer, 6,500 linear feet of proposed 12" sanitary sewer, 6,200 lf of proposed 12" watermain, traffic control plans; and a 12'x4' Conspan culvert crossing under Old Springfield Road. Additionally, old Dogleg Road and Jackson Road were realigned with the new roadway. The project was done in conjunction with a 130 acre commercial site development adjacent to the new roadway. The total construction cost was \$11 million dollars and construction commenced in the summer of 2014. It included: <b>new roadway geometrics and realignment of two existing intersecting roadways;</b> hydraulic floodplain study; <b>complex coordination with a new commercial site</b>

		<p><b>development; Right of Way impacts in restrictive site conditions;</b> and extensive utility coordination.</p> <p>Consultant Role: Prime    Service Cost: \$675,000    Construction Cost: \$11,765,000  Client: City of Union, Ohio/Montgomery County Transportation Improvement District  Client Contact: Steve Stanley (937) 226-8266</p>
2009 to 2011	<p><b>Austin Landings Road Interchange</b>  Montgomery County, Ohio</p>	<p><b>Project Manager</b> -Prepared certified traffic and updated the MVRPC model to account for the proposed Austin Landings / I-75 interchange. Prepared Visim traffic model for the Austin Road and SR. 741 corridors to analyze the local system impacts caused by 200 acres of mixed use development. Prepared design plans for 7000 lf of widening and five intersection upgrades with new mast arm traffic signals and new traffic control plans. The project also included curb and gutter, temporary signals, sidewalk, lighting, drainage improvements, utility upgrades and utility relocation. Total construction value for this project was \$9,200,000.</p> <p>It included: <b>roadway widening and right of way impacts in restrictive site conditions; complex coordination in conjunction with a new commercial site development;</b> and extensive utility coordination.</p> <p>Consultant Role: Prime    Service Cost: \$1,050,000    Construction Cost: \$9,200,000  Client: Montgomery County Transportation Improvement District  Client Contact: Steve Stanley (937) 226-8266</p>
2003 to 2013	<p><b>Kennedy Connector</b>  Cincinnati, Ohio</p>	<p><b>Project Manager</b> - The project enabled the re-development of the 75 acre Milacron brownfield property into Oakley Station, which has 300 new residences and capital improvements of \$100m. The construction of a <b>new, 5 lane road in a previously developed neighborhood involved “threading the needle”</b> to go over an USACE flood control wall; avoid two 137 kv electric towers; go over an active rail line with a two-piece pre-cast arch; and to mitigate impacts on residences while maintaining traffic. This \$21,000,000 investment included the construction of 7,100 ft of roadway, seven traffic signals, traffic control plans, lighting, two bridges, two retaining walls, storm sewers and sanitary sewers. The project separated 23.4 acres of developed area from the combined sewer system.</p> <p>It included: hydraulic floodplain study and 9 feet profile grade adjustment to accommodate USACE floodplain requirements; <b>roadway improvements and Right of Way impacts in restrictive site conditions;</b> extensive utility coordination including relocation of a 20” high pressure gas main, electric over the railroad tracks, the jack and bore of 4 locations for new water mains, sanitary sewer and combined sewer and the design of new water mains along the Kennedy Connector.</p> <p>Consultant Role: Prime    Service Cost: \$2,100,000    Construction Cost: \$21,000,000  Client: City of Cincinnati    Client Contact: John Brazina (513) 352-3720</p>
2009 to 2010	<p><b>Brandt Pike (SR 201) Improvements</b>  City of Huber Heights, Ohio</p>	<p><b>Project Manager</b>-Prepared design plans to widen Brandt Pike (SR 201) from I-70 to approximately 800 l.f. north of the west leg of Shull Road including the widening of the bridge, for a total length of approximately 3,400 lf. The final widened project ties into the proposed nine-lane widened section that will be constructed as part of the I-70/Third Lane project and taper down to two lanes in each direction with a center turn lane or grass median. The project included curb and gutter, a new signal at Shull Road, culvert extensions, lighting, traffic control plans, and sidewalks. The project helps handle the changes of traffic flow that were caused by the Old Troy Pike Interchange Project and the I-70/Third Lane Project.</p> <p>Complex issues included: <b>roadway widening and right of way impacts in restrictive site conditions; design coordination with adjoining ODOT Interstate project;</b> and extensive utility coordination.</p> <p>Consultant Role: Prime    Service Cost: \$225,000    Construction Cost: \$5,100,000  Client: City of Huber Heights    Contact: John Geiger (513) 932-3060</p>



STAFF EDUCATION & EXPERIENCE REPORT		
NAME	TITLE	ROLE ON PROJECT
Jeffrey Glowka, PE	Project Engineer	Lead Road Engineer
COMPANY NAME	YEARS WITH IBI GROUP	TOTAL YEARS OF EXPERIENCE
IBI Group	8	19
EDUCATION		
B.S., Civil Engineering, University of Cincinnati, 1998		
LICENSES / REGISTRATIONS		
Professional Engineer, Ohio, 2001, #67484; Florida, 2017, #83876; Michigan, 2018, #6201066680		
EXPERIENCE & QUALIFICATIONS		
Mr. Glowka has a strong background in design, engineering and construction project management of roadway projects. He has successfully managed and designed projects for several clients including Ohio Department of Transportation (ODOT), local municipalities and counties. Mr. Glowka's extensive experience includes; geometric design of horizontal and vertical alignments, preparing drainage studies and calculations, designing storm sewer systems, establishing construction limits, preparing roadway plans, calculating plan quantities, preparing cost estimate utilizing estimator software, and construction staging by developing appropriate maintenance of traffic plans.		
SPECIFIC EXPERIENCE		
DATES	PROJECT I.D.	PROJECT DESCRIPTION AND ACTIVITY
2003 to 2013	<b>Kennedy Connector</b> Cincinnati, Ohio	<p><b>Lead Project Engineer</b></p> <p>The project relieves congestion at the Madison and Ridge Intersection and provides direct access to SR 562 WB and I-71 SB to major employers along Duck Creek Road, including: 5/3 Bank and Coca-Cola. The construction of a <b>new, 5-lane road</b> in a previously developed neighborhood involved "threading the needle" to go over an USACE flood control wall; avoid two 137 kv electric towers; go over an active rail line with a two-piece pre-cast arch; and to mitigate impacts on residences, while maintaining traffic. In conjunction with the project, 6500 lf of 48" and 7000 lf of 16" water mains were constructed in the project corridor, including a self-supporting two span steel pipe bridge for the 48" water main. <b>Critical storm water elements included the use of detention to meet water quality requirements; the hydraulic analysis and water surface profile modeling of the new single span bridge over the Duck Creek</b> (3.62 sq. mi drainage area) using legacy HEC-2 software; and studying future development areas to appropriately size the storm sewer system. The project included four trenchless crossings (2 water, 1 sanitary sewer and 1 storm water) under a live railroad. The project separated 23.4 acres of developed area from the combined sewer system. This separation project returned storm water to an area that hadn't seen surface storm water in almost 80 years, so extensive considerations were given to scour, velocity control and channel stability. This \$21,000,000 investment included the <b>construction of 7,100 ft of roadway</b>, seven traffic signals, lighting, two bridges, relocation of major gas and electric utilities, and two retaining walls.</p> <p>Consultant Role: Prime      Service Cost: \$2,100,000      Construction Cost: \$21,000,000 Client: City of Cincinnati      Client Contact: John Brazina (513) 352-3720</p>
2008 to 2011	<b>Lunken / Armleder Multi-Use Trail and Bridge</b> Cincinnati, Ohio	<p><b>Lead Project Engineer</b></p> <p>Civil engineering and surveying services for <b>5000 lf of multi-use trail</b>. The project includes a 150 foot single span bridge over the Little Duck Creek. Hydraulic analyses were completed using HEC-RAS. The bridge is subject to three different 100 year flood scenarios: the Little Duck Creek and the Little Miami and Ohio River back-water conditions. The bridge is designed to be submerged more than 20 feet in the Ohio River flood condition. Scour analysis and scour counter measures were critical due to the silty nature of the project. As part of the scour protection, the creek channel was modified and the stream banks were rebuilt and vegetated. <b>The project required a National Scenic Rivers permit; USACE and OEPA permits for the channel work and flood plain</b></p>

		<p><b>approvals.</b> An 800' long, 8' high mechanically stabilize earth, vegetated basket wall was used instead of a traditional wall to minimize visibility from the scenic river.</p> <p>Consultant Role: Prime    Service Cost: \$1,050,000    Construction Cost: \$6,000,000</p> <p>Client: Great Parks of Hamilton County    Client Contact: Todd Palmeter (513) 728-3551</p>
2012 to 2014	<p><b>Kinsey Road (GRE-TR 88) Intersection Improvements</b></p> <p>Xenia, Ohio</p>	<p><b>Lead Project Engineer</b></p> <p>IBI provided surveying and engineering services for <b>the widening of the north side of the east leg of the intersection N Detroit (U.S. 68) /Kinsey Road west of Old Springfield Pike</b> to create a dedicated northbound right turn lane and extend the stacking queue for the southbound left turn lane. Additionally the project includes the replacement of the span wire traffic signal to a new mast arm signal. <b>The project included the completion of a Categorical Exclusion Level 1 (CE1) Document</b> and was administered by the City of Xenia using ODOT's LPA process. The project schedule was accelerated by almost six months to be eligible as a potential reservoir project.</p> <p>Consultant Role: Prime    Service Cost: \$68,800    Construction Cost: \$225,000</p> <p>Client: City of Xenia, Ohio    Client Contact: Chris Berger (937) 376-726</p>
2009 To 2014	<p><b>Spangler Road Improvement Project and Watermain Extension</b></p> <p>Fairborn, Ohio</p>	<p><b>Project Engineer</b></p> <p>IBI assisted the City of Fairborn with the reconstruction and widening of Spangler Road from just south of State Route 444 (North Broad Street) to just north of West Yellow Springs Fairfield Road (approximately 3,700 ft.) <b>The project included a vertical profile correction and proposed pavement widening with shoulders to improve safety and accommodate truck traffic to the I Supply Company,</b> a significant local distribution center. Additionally drainage and traffic control improvements were incorporated along the corridor. A sanitary sewer and watermain extension were also included as part of the project. The sanitary sewer extension was an 8" pipe jack and bored under an existing Norfolk Southern Railroad. The watermain extension consisted of approximately 4,300 lineal feet of 16" ductile iron pipe. (\$1.75 million est. construction cost). A total of 11 parcels were impacted with this project. In addition to completion of the construction documents IBI assisted the City with the acquisition of the railroad permit from Norfolk Southern Railroad and coordination with Enterprise Products, a natural gas distribution owner, to ensure embankment requirements were achieved.</p> <p>Consultant Role: Prime    Service Cost: \$42,500    Construction Cost: \$1,750,000</p> <p>Client: City of Fairborn, Ohio    Client Contact: Pete Bales (937) 754-3155</p>
2012 to 2014	<p><b>Union Airpark Boulevard</b></p> <p>City of Union, Ohio/Montgomery County</p>	<p><b>Lead Project Engineer</b></p> <p>Provided civil engineering and surveying services to the City of Union and the Montgomery County Transportation Improvement District with the construction of approximately <b>1.75 miles of new roadway located between SR 40 and Old Springfield Road.</b> The improvements included proposed roadway with curb and gutter, storm sewer, 6,500 linear feet of proposed 12" sanitary sewer, 6,200 lf of proposed 12" watermain, 11,000 lf of storm water piping and a 12'x4' precast concrete (1.29 sq. mi drainage area) culvert crossing under Old Springfield Road. Additionally, <b>old Dogleg Road and Jackson Road were realigned with the new roadway.</b> The project was done in conjunction with a 130 acre commercial site development adjacent to the new roadway.</p> <p>Consultant Role: Prime    Service Cost: \$675,000    Construction Cost: \$11,765,000</p> <p>Client: City of Union, Ohio/Montgomery County Transportation Improvement District</p> <p>Client Contact: Steve Stanley (937) 226-8266</p>

STAFF EDUCATION & EXPERIENCE REPORT		
NAME	TITLE	ROLE IN PREQUALIFICATION
Steve Butler, PE	Senior Bridge Engineer	Lead Bridge Engineer
COMPANY NAME	YEARS WITH IBI GROUP	TOTAL YEARS OF EXPERIENCE
IBI Group	10	15
EDUCATION		
B.S., Civil Engineering, West Virginia University Institute of Technology, 2002, Montgomery, West Virginia		
LICENSES / REGISTRATIONS		
Professional Engineer - MI (6201060378), Professional Engineer - OH (71774), Professional Engineer – WV (17141), Professional Engineer – CA (84432)		
EXPERIENCE & QUALIFICATIONS		
Mr. Butler has over 15 years of civil engineering experience, with emphasis in structural bridge design. He has evaluated/developed superstructure and substructure plans for a variety of bridges within the states of Michigan, Ohio, and West Virginia. His experience extends to all phases of civil engineering projects, including preliminary studies and reports, site surveys and investigations, specifications, quantity take-offs, and cost estimates.		
SPECIFIC EXPERIENCE		
DATES	PROJECT I.D.	PROJECT DESCRIPTION AND ACTIVITY
2016 to 2017	<b>I-75 NB &amp; SB over Northline Road, and I- 75 SB over US-24 Connector,</b> Wayne County, MI	<b>Key Bridge Support Engineer</b> For each of the three bridges involved with this project, the scope of work includes: <b>bridge widening, deck replacement,</b> backwall replacement, pier strengthening, pier cap replacement, structural steel repairs, substructure repairs, and approaches. The scope of work also includes replacing portion of the fill behind the existing abutments with EPS blocks in order to upgrade the existing abutments to current AASHTO standards. Responsibilities included design of the proposed steel girders and the structural evaluation of the existing girders. Client: MDOT, James Ranger, PE, 517-373-0737 Construction Cost: \$7,000,000 Design Fee: \$560,000
2009 to 2016	<b>HAM-75-1338</b> Cincinnati, OH	<b>Lead Bridge Engineer</b> This project consisted of constructing a <b>new bridge for Vision Way over I-75 and Neumann Way.</b> In addition to bridge plans, IBI Group prepared roadway, lighting, drainage, maintenance of traffic, and right of way plans along with a bridge load rating analysis. The bridge consists of a three span (111.5', 112', 93') prestressed I-Girder superstructure with a composite reinforced concrete deck supported by MSE Wall Abutments and Cap & Column type piers on piling. Client: ODOT District 8, Chris Casson, 513-857-7367
2014 to 2015	<b>HAM-CR 101-0.90 (Galbraith Road Bridge)</b> Cincinnati, OH	<b>Lead Bridge Engineer</b> This project consisted of <b>replacing a portion of the existing superstructure and rehabilitating the existing abutment and piers</b> for the Galbraith Road Bridge over the West Fork of Mill Creek. In addition to bridge plans, IBI Group prepared roadway, drainage, and maintenance of traffic plans along with a bridge load rating analysis. The bridge consists of a three span (39.25', 49.83', 39.25') prestressed box beam superstructure with a composite reinforced concrete deck supported on newly poured beam seats. Client: Hamilton County Engineer, Todd Gadbury, PE 513-946-8445

2014 to 2015	<b>Hazelwood Park Bridge</b> Springboro, OH	<b>Bridge Reviewer</b> This project consisted of constructing a <b>new bridge to access a new city park</b> over Clear Creek in the City of Springboro, OH. In addition to bridge plans, IBI Group prepared roadway, drainage plans along with a bridge load rating analysis. The bridge consists of a three span (34', 42.5', 34') concrete slab bridge supported on capped pile piers and abutments. Client: City of Springboro, Roy Henson, PE, 513-695-3301
2014 to 2014	<b>John Gray Road Bridge Replacement</b> Fairfield, OH	<b>Lead Bridge Engineer</b> This project consisted of <b>replacing the superstructure and rehabilitating the existing abutments</b> for the John Gray Road Bridge over Pleasant Run Creek. In addition to bridge plans, IBI Group performed a bridge load rating analysis. The bridge consists of a 49' long single span prestressed box beam superstructure with a composite reinforced concrete deck supported on newly poured beam seats. Client: Hamilton County Engineer, Todd Gadbury, PE 513-946-8445
2012 to 2013	<b>WA 126-1.34 (Red Lion- Five Points Rd Bridge)</b> Springboro, OH	<b>Bridge Reviewer</b> This project consisted of providing construction drawings for <b>two alternatives on the Red Lion-Five Points Road Bridge</b> over an unnamed stream to accommodate roadway widening. The bridge alternatives consisted of a 20' single span prestressed box beam superstructure widening supported on wall type abutments on spread footings and a complete replacement with a 8' x 18' box culvert. Client: City of Springboro, Roy Henson, PE, 513-695-3301
2012 to 2013	<b>AUG-MER- SHE-VAR- VAR</b> Various Locations	<b>Lead Bridge Engineer</b> This project consisted of <b>rehabilitating three existing bridges</b> in Auglaize, Mercer and Shelby Counties. The project included placing a latex modified concrete deck on the existing box beams and approach slab, expansion joint replacements, abutment repair using galvanic anodes and miscellaneous patching and painting. IBI Group also prepared roadway and maintenance of traffic plans. Client: ODOT District 7, Scott Boyer, PE, 937-497-6807
2012 to 2013	<b>CHP-296- VAR</b> Champaign County, OH	<b>Bridge Reviewer</b> This project consisted of <b>rehabilitating four existing bridges</b> in Champaign County on SR 296. The project included pile encasement for capped pile piers, expansion joint replacements, deck edge repair using galvanic anodes, abutment repair using galvanic anodes and miscellaneous patching. IBI Group also prepared roadway, environmental, and maintenance of traffic plans. Client: ODOT District 7, Scott Boyer, PE, 937-497-6807
2011 to 2012	<b>SCI-73-01.10 Bridge</b> Scioto County, OH	<b>Bridge Reviewer</b> This project consisted of <b>constructing a new bridge for S.R. 73</b> over Rarden Creek on new curved alignment. In addition to bridge plans, IBI Group prepared roadway, drainage, maintenance of traffic, right of way, and demolition plans. The bridge consists of a three span (28', 35', 28') reinforced concrete slab superstructure supported by reinforced concrete stub abutments and capped pile piers. Design complexities included the reuse of many of the existing bridge elements and part width construction detailing. Client: ODOT District 9, Matthew R Hauck, PE, 740-774-9055



STAFF EDUCATION & EXPERIENCE REPORT		
NAME	TITLE	ROLE ON THIS PROJECT
<b>Greg Jackman, PE</b>	Bridge Engineer	<b>Bridge Design Engineer</b>
COMPANY NAME	YEARS WITH IBI GROUP	TOTAL YEARS OF EXPERIENCE
<b>IBI Group</b>	6	6
EDUCATION		
B.S. Civil Engineering, Lawrence Technological University, 2009, Southfield, Michigan		
LICENSES / REGISTRATIONS		
Professional Engineer – Michigan (6201063296) Professional Engineer – Ohio (82727)		
EXPERIENCE & QUALIFICATIONS		
As a Structural Engineer, Mr. Jackman is responsible for actively leading all bridge load rating assignments. He has load rated scores of bridges, is deeply familiar with AASHTOWare BrR software and has trained other engineers on the software and MDOT policies and procedures. Mr. Jackman also assists in the design and analysis of numerous types of bridge components, leading efforts in preparation of final project plans, preparing quantities, reviewing shop drawings and contributing to construction assistance.		
SPECIFIC EXPERIENCE		
DATES	PROJECT I.D.	PROJECT DESCRIPTION AND ACTIVITY
2015	<b>Cathedral Avenue Walkover over M-39,</b> Wayne County, MI (CS 82192 - JN 126931)	<b>Key Bridge Support Engineer</b> The IBI team was selected for the design of this bridge under an “as-needed” 2 stage selection process. This project included the fast-track replacement of a pedestrian bridge with significant aesthetic detailing & coordination. The new bridge included a two span bridge with prestressed concrete box beams and 520 feet of pedestrian ramps with an innovative design concept. Through team work and a commitment for quick resolution of issues (hours instead of days), preparing the final bid package took only two and a half months. This project won the <b>MDOT / ACEC 2017 Partnership Charter Award</b> Client: MDOT, Jose Garcia, PE, 517-373-0075 Construction Cost: \$2,500,000, Design Fee: \$225,000
2013 to 2015	<b>I-75 from Dixie Highway to Hess Road,</b> Bay County, MI (CS 73111 - JN 109066)	<b>Key Bridge Support Engineer</b> This project included the reconstruction of 1.75 miles of I-75 Freeway and the replacement of three bridges with their approaches. The existing bridges were four span steel grade separation bridges that needed to be replaced to allow for widening the freeway underneath. They were replaced with two span integral bridges with prestressed concrete beams. The abutments were stub abutments founded on single line of steel H-piles behind full height MSE walls. This project won the <b>Mid America’s Transportation 2016 Award for Under Budget – Medium Category (projects 26 million-199million).</b> Client: MDOT, Clint Mayoral, PE, 517-241-1130
2013 to 2014	<b>SB Oakland Avenue over M-8,</b> Wayne County, MI (CS 82104 - JN 119174)	<b>Key Bridge Support Engineer</b> This project consisted of rehabilitating a 5 span steel I-beam structure. The bridge work included deck replacement, north backwall replacement, repositioning one span, cleaning & coating structural steel, substructure repairs, concrete surface coating and pedestrian fencing. Client: MDOT, James Ranger, PE, 517-373-0737 Construction Cost: \$2,100,000. Design Fee: \$249,000

2014 to 2016	<b>Evergreen Road over I-96 &amp; CSX RR,</b> Wayne County, MI (CS 82122 - JN 122645)	<b>Key Bridge Support Engineer</b> This project was the rehabilitation of one of the big ten bridges in Michigan, a 19 span steel beam bridge with pin & hanger assemblies. The rehabilitation work included, a deep concrete overlay, transverse deck joint replacement, partial sidewalk replacement, railing and fencing repairs, railing retrofit at light poles, superstructure retrofit at proposed signal poles, pin & hanger repairs, and substructure repairs. Special construction sequencing details were developed for 2,700 linear feet of sidewalk replacement to remediate a potential unsafe condition in the adjacent existing deficient barrier construction. Client: MDOT, Kyle Kopper, PE, 517-373-0736 Construction Cost: \$5,000,000 Design Fee: \$250,000
2016 to 2017	<b>Chene Road over I-94,</b> City of Detroit, Wayne County, MI (CS 82024 - JN 113124)	<b>Key Bridge Support Engineer &amp; Lead Load Rating Engineer</b> This project included the reconstruction of the Chene Bridge over I-94 and its approaches. The existing bridge was a four span steel grade separation bridge that needed to be replaced to allow for future widening of the freeway underneath. The proposed bridge is a two span bridge with full height abutments and continuous steel plate girders. The proposed bridge had to be designed and rated for the interim conditions and the future conditions when the pier have to be relocated 24' toward the south. Client: MDOT, Tim Barry, PE, 517-335-7275 Construction Cost: 10,000,000 Design Fee: \$1,000,000
2016 to 2017	<b>I-75 NB &amp; SB over Northline Road, and I-75 SB over US-24 Connector,</b> Wayne County, MI (CS 82191 - JN 122199)	<b>Key Bridge Support Engineer &amp; Lead Load Rating Engineer</b> For each of the three bridges involved with this project, the scope of work includes: bridge widening, deck replacement, backwall replacement, pier strengthening, pier cap replacement, structural steel repairs, substructure repairs, and approaches. The scope of work also includes replacing portion of the fill behind the existing abutments with EPS blocks in order to upgrade the existing abutments to current AASHTO standards. The existing bridges are three and four span steel bridges. This project also involved rating 11 bridges with steel beams. Client: MDOT, James Ranger, PE, 517-373-0737 Construction Cost: 7,000,000 Design Fee: \$560,000

STAFF EDUCATION & EXPERIENCE REPORT		
NAME	TITLE	ROLE ON PROJECT
Erin E. Wilkinson, EIT	Project Engineer	Road Modeling and GeoPak
COMPANY NAME	YEARS WITH IBI GROUP	TOTAL YEARS OF EXPERIENCE
IBI Group	1	9
EDUCATION		
Bachelor of Science in Civil Engineering, 2008, Wayne State University (Detroit, Michigan)		
LICENSES / REGISTRATIONS		
Professional Engineer - EIT, 2009, State of Texas, License Number: 42258		
EXPERIENCE & QUALIFICATIONS		
Ms. Wilkinson works with a team of engineers designing roadways for municipalities, counties, and MDOT. In her role as a project engineer, she serves as a designer, responsible for all aspects of road design. Ms. Wilkinson is familiar with MDOT, MDOT Local Agency Program (LAP), and numerous county/municipal plan preparation procedures from the project kick-off through the final plan submittal. Her experience in plan preparation and design ranges from local 3R rehabilitation projects to major interstate reconstruction projects. She has extensive knowledge of the MDOT road design procedures, standards, and specifications. She also has extensive experience in complex geometric design, including complex grading, ADA compliance, and cross sections design.		
SPECIFIC EXPERIENCE		
DATES	PROJECT I.D.	PROJECT DESCRIPTION AND ACTIVITY
2015 to Ongoing	<b>John R Road Reconstruction; Long Lake Road to South Blvd.</b> Troy, Michigan	<b>Road Designer</b> Road designer for the reconstruction of approximately two miles of urban arterial pavement to widen from two lanes with intermittent passing lanes to a full three lane roadway. The TEDF funded project is estimated at \$18 million and includes replacement of the bridge deck over the Nelson Drain, water main, storm sewer, sidewalks, traffic signal modernization, lighting, signing pavement marking and site restoration. The project also included a supplement to update the original Environmental Assessment, which was completed in 2003. Work included coordination with MDOT, MDEQ, RCOC, City of Rochester as well as all utility companies in the corridor for utility relocations and conflict avoidance. Construction is estimated to cost \$15 million and is anticipated to occur in 2018 – 2019. <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$279,000 <b>Construction Cost:</b> \$3,700,000 <b>Client:</b> City of Troy <b>Client Contact:</b> Mr. Bill Huotari, PE, (248) 524-3387
2016 to Ongoing	<b>US-24 (Telegraph Road), from Buhl Road to Ready Road</b> Monroe County, Michigan	<b>Roadway Engineer</b> Roadway Engineer for ½ mile of HMA pavement widening and 3 ½ miles of CPM on US-24 in Frenchtown and Ash Townships. US-24 will be widened from 4 to 5 lanes south of Newport Road, including replacement of a box culvert with a new reinforced concrete elliptical pipe. CPM work will include cold milling and a one-course HMA resurfacing on US-24, including the ramps at the I-275 interchange. The projects also includes signing, pavement markings, maintaining traffic and a temporary signal. <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$702,500 <b>Construction Cost:</b> \$18,552,500 <b>Client:</b> MDOT-Brighton TSC <b>Client Contact:</b> Ms. Lynne Kirby, PE (810) 255-2627
2016 to 2016	<b>Five Bridges Over I-94</b> Berrien County, Michigan	<b>Design Engineer</b> Design Engineer for this project to provide design services for bridge approaches at five roadways over I-94. Project includes the bridge deck rehabilitations including approach work, drainage, signing, pavement marking and maintenance of traffic plans and provisions. MDOT Bridge Design Unit is preparing bridge plans. Project includes coordination with bridge unit, utility coordination, permitting coordination, roadway design, drainage design, guardrail design. <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$87,980 <b>Construction Cost:</b> \$490,000 <b>Client:</b> MDOT-Marshall TSC <b>Client Contact:</b> Mr. Dharmesh Valsadia, PE (269) 789-0560

2014 to 2016	<b>I-75 APB Rehabilitation from Cottage Grove to Linwood</b> Bay County, Michigan	<b>Design Engineer</b> Design Engineer responsible for the preparation of two independent sets of plans for this alternate pavement project. The project involves a rubbilize & HMA overlay as well as an un-bonded concrete overlay design. A segment of I-75 will be reconstructed under Linwood Road to achieve underclearance and the four ramps at the Linwood interchange will be crushed and shaped with an HMA overlay. All culverts are being replaced along with ditching design to improve roadside drainage. Project also includes the replacement of the ROW fence along the length of the job. <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$1,000,000 <b>Construction Cost:</b> \$12,500,000 <b>Client:</b> MDOT-Bay City TSC <b>Client Contact:</b> Mr. Ryan McDonnell, PE (989) 671-1555
2012 to 2015	<b>M-24 Rehabilitation, Harmon Road to Clarkston Road</b> Auburn Hills and Orion Township, Michigan	<b>Design Engineer</b> Design Engineer for OHM to design two miles of the project to rehabilitate the boulevard roadway between Harmon Road and Silverbell Road. Design elements included vertical and horizontal improvements, drainage improvements and reconstruction of the Silverbell Road intersection to improve safety as well as other safety improvements derived from a Road Safety Audit recommendations. Design services also include seven traffic signals, a pathway analysis and a complete non-motorized pathway system. <b>Consultant Role:</b> Sub <b>Service Cost:</b> \$634,341 <b>Construction Cost:</b> \$23,300,000 <b>Client:</b> MDOT-Wade Trim <b>Client Contact:</b> Mr. Jeff Piff, PE (248) 451-2430 Mr. Matt Stacey, PE (734) 947-2678
2012 to 2015	<b>M-24 Reconstruction</b> City of Lapeer, Michigan	<b>Road Design Engineer</b> Road Design Engineer responsible for extensive design of M-24 in Lapeer. The project consists of two miles of reconstruction with numerous intersections, freeway exit ramps, railroad crossing, culvert replacement, and several utility and ROW conflicts. The project requires vertical and horizontal alignments along with detail grading of the roadway especially at numerous intersections along with detail grading of the sidewalk along the entire corridor of the project to make the sidewalk ADA compliant. Designed superelevation and cross sections using Geopak as a tool for the design. Calculated numerous quantities including earthwork quantities for the project. <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$807,118 <b>Construction Cost:</b> \$10,500,000 <b>Client:</b> MDOT <b>Client Contact:</b> Ms. Jacqueline Pethers (810) 658-4029
2012 to 2013	<b>West Grand Boulevard at I- 94 Bridge Replacement,</b> City of Detroit, Michigan	<b>Design Engineer</b> Design Engineer where OHM provided design services to MDOT in developing design plans for the road portion of the bridge replacement project. Responsible for creating cross sections for West Grand Blvd along with I-94 using Geopak. <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$280,000 <b>Construction Cost:</b> \$17,000,000 <b>Client:</b> MDOT <b>Client Contact:</b> Mr. Andy Hodges (248) 483-5209
2012 to 2013	<b>Gahanna Safe Routes to School (SRTS),</b> City of Gahanna, Ohio	<b>Road Design Engineer</b> Road Design Engineer responsible for creating cross sections for the road and sidewalk design. The project involves a variety of sidewalk improvements in the vicinity of four schools in the City of Gahanna. Improvements include adding new sidewalk to fill in sidewalk gaps and ADA sidewalk ramp upgrades. The project will go through the Ohio LPA process. <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$106,832 <b>Construction Cost:</b> \$N/A <b>Client:</b> City of Gahanna <b>Client Contact:</b> Mr. Mike Andrako (614) 342-4050



STAFF EDUCATION & EXPERIENCE REPORT		
NAME	TITLE	ROLE ON PROJECT
Brian Sedziol, PE, PTOE	Project Engineer	Roadway Design Engineer
COMPANY NAME	YEARS WITH IBI GROUP	TOTAL YEARS OF EXPERIENCE
IBI Group	7	7
EDUCATION		
B.S., Civil Engineering, University of Cincinnati, 2010		
LICENSES / REGISTRATIONS		
Professional Engineer, OH #80117		
EXPERIENCE & QUALIFICATIONS		
<p>Mr. Sedziol has experience preparing roadway and traffic control plans and completing traffic studies. He also has experience in traffic signal design and timing, traffic data collection, roundabout design, capacity analyses, and safety studies. Specializing in the design of roadway projects, Mr. Sedziol's responsibilities have included signage, pavement markings, signal design, and maintenance of traffic. He also has direct experience with horizontal and vertical alignments, profiles, and cross sections.</p>		
SPECIFIC EXPERIENCE		
DATES	PROJECT I.D.	PROJECT DESCRIPTION AND ACTIVITY
2008 to 2017	<b>HAM-75-12.60 Phase 1 (PID 82288) &amp; Neumann Way Phase 2 (PID 88135)</b> Hamilton County, Ohio	<p><b>Project Engineer-</b> HAM-75-12.60, PID 82288 &amp; Neumann Way, PID 88135) consists of widening and reconstruction of 2.7 miles of I-75 between the general limits of Shepherd Lane to Glendale-Milford. In addition, <b>2.1 miles of local roads and interstate ramp improvements</b> were designed. Specific plan development elements include: <b>roadway reconstruction, maintenance of traffic, 3 bridge structures, 7 retaining walls, drainage, lighting, survey, right-of-way, 10 signals, 1 noise wall, plan quantities, 3 roundabouts, utility coordination</b>, pavement marking and project management. <u>Complex items</u> included: geometric studies and IMS to modernize two interchanges and provide missing movements; addition of one southbound &amp; northbound lane with auxiliary exit/entrance lanes in restrictive site conditions; municipal utility relocations including impacted electric, sanitary sewer, communications, domestic and process water facilities; elimination of I-75 C-D road with at-grade driveway access points and development of a two-way local road with 3 multi lane roundabouts to provide access to an adjacent General Electric Jet Engine factory; profile grade adjustments to provide required vertical clearance at bridge structures; extensive drainage work including over 37 miles of pipe underdrain &amp; 8 miles of storm sewer conduit; provisions for best management practices (water quality); major right of way impacts including 52 total takes and 118 parcels; 9-phase MOT Scheme including temporary roads to maintain access to the General Electric Jet Engine factory; and aesthetic enhancements to bridge and wall structures.</p> <p><b>Consultant Role:</b> Prime <b>Service Cost:</b> \$4,200,000 <b>Est. Construction Cost:</b> \$94,000,000  <b>Client:</b> Ohio DOT District 8 <b>Client Contact:</b> Chris Carson (513) 933-6588</p>
2005 to 2011	<b>East Orange Road Improvements</b> Delaware County, Ohio	<p><b>Project Engineer -</b> Design improvements to East Orange Road from South Old State Road east to Bale-Kenyon Road (8100 ft.) <b>Completed a traffic study to examine the need for roadway widening throughout the project limits and to analyze any traffic and/or safety concerns</b> associated with the existing and proposed schools; location of the proposed bike path, and possible drive locations for the proposed high school. Based on this study. IBI Group evaluated the use of roundabouts at three locations which saved approximately \$300,000 as compared to signals. The project included full depth pavement, pavement widening, three sided culvert, and drainage provisions. <u>Complex items</u> included: <b>geometric and capacity analyses for the conversion of two existing intersections into 1-lane roundabouts, plus a third intersection into a multi-lane roundabout; profile grade adjustment to improve safety in a highly restrictive area (next to a high school and middle school); provisions for best management practices (water quality); major right of way impacts including 74 parcels; and hydraulic floodplain study for one bridge.</b></p> <p><b>Consultant Role:</b> Prime <b>Service Cost:</b> \$500,000 <b>Est. Construction Cost:</b> \$5,000,000  <b>Client:</b> Delaware County Engineer, Ohio <b>Client Contact:</b> Ryan Mraz (740) 833-2425</p>

2011 to 2016	<b>STA-153-12th Street Corridor Improvements</b> Canton, Ohio	<b>Project Engineer</b> - The project was a <b>2.2-mile corridor with 13 major signalized intersections</b> , including the Market Avenue intersection, then ranked as Stark County's most dangerous. The project added left turn lanes on Market Avenue and converted the geometrically awkward 12th Street/O'Jays Avenue/Maple Avenue signalized intersection to a 1-lane roundabout. IBI assisted the City in obtaining a total of \$20.3M in funding from HSP, CMAQ, Enhancement, and OPWC funding sources. Complete street elements were incorporated including <b>bike lanes, bus stops, and decorative elements such as street lighting, street trees with decorative sidewalk, and decorative pavement at select intersections</b> . The project was bid in the summer of 2014 with a low bid of \$16.45M. Construction was completed in September 2016. <b>Complex items included: geometric studies for the conversion of the awkward 12th Street/O'Jays Avenue/Maple Avenue signalized intersection to a single-lane roundabout; alignment layout studies to accommodate complete street principles; profile grade adjustments to improve safety in a highly restrictive urban area; extensive urban storm sewer improvements including provisions for best management practices (water quality); major right of way impacts including 240 parcels; and multi-phase Maintenance of Traffic Scheme.</b> <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$2,100,000 <b>Est. Construction Cost:</b> \$16,450,000 <b>Client:</b> City of Canton <b>Client Contact:</b> Nicholas Loukas, PE (330) 489-3283
2016 to 2018	<b>Lewis Center Road Roundabout</b> Delaware County, Ohio	<b>Project Engineer</b> - Improvements at the intersection of Lewis Center Road and North Road in Delaware County, Ohio convert an existing stop controlled "T" intersection into a four-leg roundabout. The fourth leg is provided to accommodate future residential development. <b>The project aims to abate safety concerns by incorporating successive horizontal entry curves and profile grade adjustments.</b> Turn lanes and shared use paths are included to address the needs of an adjacent high school property. Drainage issues presented by a creek running beneath the intersection are resolved with a new culvert under the proposed roundabout. IBI Group has submitted Stage 3 plans to Delaware County, and construction is scheduled to begin in late 2018. <b>Complex items included: reducing high entry speeds by incorporating successive horizontal entry curves and profile grade adjustments.</b> <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$600,000 <b>Est. Construction Cost:</b> \$3,700,000 <b>Client:</b> Delaware County Engineers <b>Client Contact:</b> Roy Riley (740) 833-2431
2014 to 2016	<b>LAW-52/Solida Road Roundabout – Ohio Transportation Improvement District</b> Lawrence County, Ohio	<b>Project Engineer</b> - IBI was contracted by the Lawrence County Transportation Improvement District to develop and <b>analyze alternatives for improvements to the diamond interchange of US 52 and Solida Road</b> near an expanding industrial park in South Point, Ohio. IBI analyzed alternatives including turn lanes and traffic signals and a double roundabout option which became the recommended improvement. This interchange has closely spaced frontage roads on both sides and these were incorporated into the roundabout option on either side of the interchange. IBI prepared an Interchange Modification Study (IMS) for ODOT approval. The IMS determined that converting from a conventional diamond interchange with adjacent frontage road intersections to a double roundabout design did not degrade the operation of the freeway. IBI also prepared Stage 1 plans for the <b>double roundabout design</b> to allow the client to continue the project as a Design-Build project in which the design build team will complete the plans. <b>Complex items included: geometric studies which ultimately led to incorporating the closely spaced frontage roads on both sides of the interchange into the double roundabout option.</b> <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$210,000 <b>Est. Construction Cost:</b> \$2,500,000 <b>Client:</b> Lawrence County Engineer <b>Client Contact:</b> Patrick D. Leighty, PE, PS (740) 533-4317
2011 to 2012	<b>CLA-41-30.98 Roundabout</b> Clark County, Ohio	<b>Project Engineer</b> - IBI was selected to design through Stage 1 design plans a <b>high speed single lane roundabout. In this case, two high speed rural state routes with a severe skew (69 degree) had experienced many crashes and fatalities.</b> The design had to consider how to reduce the entry speeds, as well as consider maintenance of traffic, minimize right of way impact, while improving substandard vertical profile and horizontal alignment. Our off-center design reduced the entry speeds through successive curves and benefited maintenance of traffic and construction sequencing while minimizing right of way impact. A 3-D model was also provided for ODOT's public meeting on the recommended alternative. <b>Complex items included: reducing high entry speeds while minimizing right of way impact, consider maintenance of traffic while improving substandard vertical profile and horizontal alignment.</b> <b>Consultant Role:</b> Prime <b>Service Cost:</b> \$40,000 <b>Est. Construction Cost:</b> \$1,500,000 <b>Client:</b> Ohio DOT District 7 <b>Client Contact:</b> Scott Boyer (937) 497-6807

## Staff Education and Experience Report

### EMPLOYEE NAME

Jeffrey D. Bartlett, P.S.

### TITLE

Project Surveyor

### ROLE ON THIS PROJECT

Project Surveyor

### COMPANY NAME

Surveying Solutions, Inc.

### YEARS OF EXPERIENCE

16 with company; 6 with other firms

### EDUCATION: degree, year, school (Inc. city and state of school)

B.S., 1997, Surveying Engineering, Ferris State University, Big Rapids, MI

### LICENSES AND REGISTRATIONS

Professional Surveyor, 2001, Michigan, # 47943

## Specific Project Experience

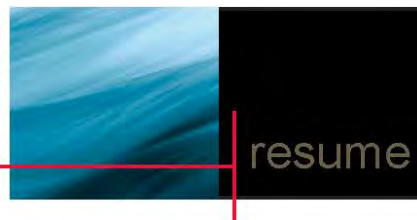
	Project ID	Role & Project Description
Dates: 01/2017 – 05/2017 Project: <b>I-75 Mega Project (Wattles to Coolidge)</b> Location: <b>Oakland County, MI</b> Project ID: <b>CS: 63174 / JN: 122317C</b> Service Budget: \$138,243 Vendor Role: Survey Consultant Client: WSP Michigan, Inc. Contact: Edwin Tatum, P.E. (313) 963-5760 MDOT Contact: Tom Tellier, P.E. (616) 464-7716		<b>Personal Role: Project Surveyor</b> Description: Project included Road Design, Right of Way (ROW), Structure, and Hydraulic Surveys for the I-75 Mega Project from (Wattles to Coolidge). <b>Hydraulic Survey</b> tasks were completed at seven locations. Upstream and downstream cross sections were collected at the locations specified MDOT Hydraulic engineer. The sections proceeded from left to right as looking downstream. River Bottoms, banks, vegetation break points, and terrain breaks were all collected. Water surface elevation was also measured. HYDRO cross sections were labeled and cross sections were connected. A HEC-RAS compatible file and a MicroStation/GEOPAK V8 file of the Hydraulics Survey was delivered utilizing MDOT Feature Codes and showed the relationship of the cross sections to the structure and the road, and noting the distance between cross sections.
Dates: 08/2015 – 04/2016 Project: <b>I-275 Rehab; 5 Mile Rd to I-96/ I-696 Interchange</b> Location: <b>Wayne &amp; Oakland Counties</b> Project ID: <b>CS: 63022, 63191, 63192 &amp; 82125 / JN: 117602C</b> Service Budget: \$44,000 Vendor Role: Survey Consultant Client: Bergmann Associates Contact: Keith Simons, P.E. (517) 272-9835 MDOT Contact: Ishrat Jahan, P.E. (248) 794-3936		<b>Personal Role: Project Surveyor</b> Description: Provided Road Design, ROW, and Hydraulic Surveys along the I-275 corridor for the purpose of a concrete inlay. <b>Hydraulic Survey</b> tasks included upstream and downstream cross sections collected at the locations specified MDOT Hydraulic engineer. The sections proceeded from left to right as looking downstream. River Bottoms, banks, vegetation break points, and terrain breaks were all collected. Water surface elevation was also measured. HYDRO cross sections were labeled and cross sections were connected. A HEC-RAS compatible file and a MicroStation/GEOPAK V8 file of the Hydraulics Survey was delivered utilizing MDOT Feature Codes and showed the relationship of the cross sections to the structure and the road, and noting the distance between cross sections.

<p>Dates: 10/216-12/2016  Route: <a href="#">M-94 / East Branch Chocoley River</a>  Location: <a href="#">Marquette County, MI</a>  Project ID: <a href="#">CS: 52022 / JN: 130242D</a>  Service Budget: \$26,000  Vendor Role: Survey Consultant  Client: MDOT - Lansing Design Unit  Contact: Karl Brandys, P.S.,  (517) 373-0020</p>	<p><b>Personal Role: QA/QC Surveyor</b>  Description: Due to washed out culverts and a section of M-94 at its intersection with the East Branch Chocoley River, Road Design, Right of Way (ROW) and Hydraulic Surveys were performed. This project was for the design of a new bridge (B01 of 52022) on M-94 to span the East Branch of the Chocoley River. <b>Hydraulic Survey</b> tasks were completed at twelve cross sections upstream and downstream of the structure. The sections proceeded from left to right as looking downstream. River Bottoms, banks, vegetation break points, and terrain breaks were all collected.</p>
<p>Dates: 02/2015 – 01/2016  Project: <a href="#">M-46 from West Village Limits of Merrill easterly to M-52 Saginaw County, MI</a>  Location: <a href="#">Saginaw County, MI</a>  Project ID: <a href="#">CS: 73061 / JN: 116505</a>  Service Budget: \$165,000  Vendor Role: Survey Consultant  Client: Bergmann Associates  Contact: Keith Simons, P.E.  (517) 272-9835  MDOT Contact: Jason Garza, P.E.,  (989) 671-1535</p>	<p><b>Personal Role: Project Surveyor</b>  Description: Provided Road Design, Right of Way (ROW), and Hydraulic Surveys in various areas along M-46 in Saginaw County. <b>Hydraulic Survey</b> field work consisted of hydraulic cross-sections taken at four stream crossings along the project area of M-46 (Wilson Drain-C01 of 73061; Whitmore Drain-C02 of 73061; Munholland Drain and McClelland Drain-C03 of 73061). Upstream and downstream cross sections were collected at the locations specified MDOT Hydraulic engineer. The sections proceeded from left to right as looking downstream. River Bottoms, banks, vegetation break points, and terrain breaks were all collected. Water surface elevation was also measured. HYDRO cross sections were labeled and cross sections were connected.</p>
<p>Dates: 04/2013 – 07/2013  Route: <a href="#">M-37 over Burkett &amp; Unnamed Creek</a>  Location: <a href="#">Wexford County, MI</a>  Service ID: <a href="#">CS: 83013 / JN: 117996</a>  Service Budget: \$26,606  Vendor Role: Survey Consultant  Client: MDOT Lansing Design Unit  Contact: Thomas Bogren, P.S.  (517) 335-1914</p>	<p><b>Personal Role: Project Surveyor</b>  Description: Provided Road Design and Hydraulic Surveys on M-37 Burkett Creek and an Unnamed Tributary. Mapping included guardrail, curb and gutter, as well as any and all surface manifestations of utilities, and structure details for culvert sizes, inverts, condition and photos. <b>Hydraulic Survey</b> field work consisted of multiple hydraulic cross-sections taken along the Burkett Creek and the Unnamed Creek at locations discussed and determined by the MDOT Project Engineer. The sections proceeded from left to right as looking downstream. Water surface elevations were also measured. All mapping performed was merged with the mapping from previous MDOT Project JN90234C.</p>



## Michael C. Gentner, P.E.

Senior Professional



Michael Gentner is a Senior Engineer/Manager with 25 years of senior leadership and demonstrated ability to manage technical and support staff, and apply practical, cost-effective engineering solutions. His core technical strengths are in Geotechnical Engineering, Earth Retention Design, and Geotechnical Instrumentation. Mr. Gentner has project experience in 15 States, Puerto Rico, and Canada.

### PROJECT EXPERIENCE

#### Geotechnical Engineering

**Main Street Bridge, Spartan Specialties, Manchester, MI.** Lead Geotechnical Engineer. Foundation testing for micropiles on a bridge replacement project. Responsible for acceptance testing and proof testing following MDOT and FHWA procedures and approving the test results prior to production piles being installed. Also provided design for the load test frame used during the tests.

**Downtown Development Property, BuildTech, Detroit, MI.** Lead Geotechnical Engineer. Completed the geotechnical evaluation report for a proposed development in downtown Detroit. The proposed building is a planned 15 story building with a 5-level parking deck. Soil borings to 110 feet included rock coring and specialty in-situ pressuremeter testing for determining the strength and compressive properties of the glacial till layer. Geotechnical report included recommendations for drilled shafts.

**15 Mile Road Sewer Collapse, AEW, Inc., Fraser, MI.** Project Manager. Provided specialty engineering consulting for emergency low mobility compaction grouting at a major sewer collapse. The 11-foot diameter CSO is 50 feet below ground surface and services 300,000 people and failed on December 24<sup>th</sup>. The Governor of the State of Michigan declared the site an emergency disaster area. A sinkhole approximately 150 feet by 350 feet and up to 9 feet deep developed, and forced evacuation of 25 homes and condemnation of 2 homes. By January 6<sup>th</sup>, GEI had developed a plan and mobilized with a specialty contractor for compaction grouting to stabilize the two ends of the sinkhole to prevent further failure and unraveling along the sewer line. The grouting plan included primary and secondary holes, with confirmation borings to show tertiary holes were not needed. Instrumentation including inclinometers, vibrating wire piezometers, and telltales were used to provide an early warning for injected grout causing stress on the tunnel.

**CP Rail Tunnel, CP Rail, Detroit, MI.** Geotechnical Engineer. Provided preliminary geotechnical engineering report for a planned freight tunnel beneath the Detroit River between the US and Canada. Soil borings were completed on land along the proposed alignment for a 30-

### EDUCATION

B.S., Civil Engineering, Michigan State University, East Lansing

### EXPERIENCE IN THE INDUSTRY

25 years

### EXPERIENCE WITH GEI

3 year(s)

### REGISTRATIONS AND LICENSES

Professional Engineer, MI No. 6201044370  
Professional Engineer, OH No. 69089  
Professional Engineer, IL No. 062061831

### TRAINING

Driver Safety Training  
Harassment Prevention Training  
Health & Safety Training – Annual

foot diameter tunnel. Soil borings included in-situ vane shear testing in soft clay, rock coring, and packer testing for rock permeability evaluation, and vibrating wire piezometers.

**ProMedica Hospital Expansion, Mannik and Smith Group, Toledo, OH.** Project Manager. Specialty foundation design for a hospital expansion. The planned construction included a 5-story parking structure with one level below ground and a 13-story patient tower with a basement. The drilled shaft and earth retention contractors requested a review of the Owner's geotechnical report and recommendations. Completed additional borings that included specialty in-situ vane shear and pressuremeter testing. This allowed higher bearing capacities and shear strength values of upper soft clay that led to more economical foundation and earth retention designs. Project savings of \$800,000 was realized using the revised geotechnical evaluation.

**World Expo Center, Adrian Smith + Gordon Gill, Anista, Kazakhstan.** Geotechnical Engineer. Provided consulting for foundation concepts for several planned structures for the expo village. Interviewed and vetted specialty foundation contractors for pre-qualification. Provided initial foundation recommendations for auger cast and driven piles. Developed specifications for foundation installation including a comprehensive load test program.

**Greenlee Residence, Dickenson Wright, Lake Angelus, MI.** Lead Geotechnical Engineer. Geotechnical design for a 30-foot bluff for a new mansion lake property. Designed a multi-tier segmental block wall.

**Ford Press Pit, IBI Group, River Rouge, MI.** Lead Geotechnical Engineer. Foundation design and recommendations for support of excavation for a new press pit within an existing and operations stamping plant. Low overhead clearance and soft clay soils were 2 factors considered for design parameters and constructability.

**Zug Island Industrial Wastewater Treatment Plan, DTE Energy, Detroit, MI.** Lead Geotechnical Engineer. Foundation design and existing foundation re-use. An existing 30-foot-diameter tank was founded on concrete piles with no as-built records and a new 45-foot-diameter tank with a larger capacity was built within the same footprint. Worked closely with the structural engineer to evaluate the existing piles condition and capacity, and provided recommendations for adding new piles. Two soil borings to bedrock were completed, along with non-destructive dynamic tests on the existing piles after the upper 5 feet were exposed. High-strain dynamic tests were completed on three instrumented test piles. Static load tests for both new steel H-piles and the existing concrete piles included vertical, horizontal, and combined horizontal with a vertical load. Developed the specifications for the pile foundations, including the load test program. All existing piles were able to be incorporated into the new tank foundation design at significant cost savings to the owner.

**Library Lot Parking Deck, Haengel and Associates Engineering, Ann Arbor, MI.** Lead Geotechnical Engineer/Project Manager. Subsurface exploration and geotechnical engineering analysis. Development may also include constructing a mid-rise building (up to 25 stories) above the parking structure. Column loads could reach 3,000 kips. Completed 5 borings to depths of 100 feet, and completed in-situ pressuremeter testing in one boring. The pressuremeter tests were completed to profile the underlying compact to very dense silts and sands below approximately 60 feet. Excavation depth for the parking structure could reach 40 feet. The design is to excavate for a near "zero clearance" to existing properties and streets. Nearby properties include low-rise (3 to 4 story) apartments/condominiums and single-story buildings. Included recommendations for earth retention systems (including soldier pile and lagging, diaphragm walls) with tieback and internal braces and rakers as temporary support options.

**New Oil Storage Tank Farm, SemMaterials, Chicago, IL.** Lead Geotechnical Engineer. Provided subsurface exploration and geotechnical engineering analysis for an upgrade to the existing marine oil terminal. The expansion consisted of constructing steel above ground tanks up to 60 feet in diameter, five smaller diameter tanks (15-foot-diameter), a two-story process building, and several light-loaded ancillary structures. Foundation design recommendations for all structures were given, as well as for site work and pavement. In-situ

## Budget Review

IBI will provide construction estimate for each of the four alternatives. Then for the selected alternative, IBI will update the estimate at the 50%, 90% and 100% stages. If the Project budget is less than the projected cost, IBI will inform Mr. Kulongowski and provide suggestions on how to meet the project budget.







# Part - I

## Section - F

References



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
**Stony Creek Metropark**

# References

- **MDOT Project Manager: Jose Garcia, PE**
  - Telephone: 517-373-0075
  - Client: MDOT
  - Project: Cathedral Avenue Walkover over M-39, Wayne County, MI; 2015
    - Site: Pedestrian Bridge over Urban Interstate
    - This project included the fast-track replacement of a pedestrian bridge with significant aesthetic detailing & coordination. The new bridge included a two span bridge with prestressed concrete box beams and 520 feet of pedestrian ramps with an innovative design concept. Through team work and a commitment for quick resolution of issues (hours instead of days), preparing the final bid package took only two and a half months. This project won the *MDOT / ACEC 2017 Partnership Charter Award*.
- **MDOT Project Manager: Kyle Kopper, PE**
  - Telephone: 517-373-0736
  - Client: MDOT
  - Project: Evergreen Road over I-96 & CSX Railroad, Wayne County, MI; 2014 to 2016
    - Site: Local Road Bridge Rehabilitation over Urban Interstate & Railroad
    - This project was the rehabilitation of one of the big ten bridges in Michigan, a 19 span steel beam bridge with pin & hanger assemblies. The rehabilitation work included, a deep concrete overlay, transverse deck joint replacement, partial sidewalk replacement, railing and fencing repairs, railing retrofit at light poles, superstructure retrofit at proposed signal poles, pin & hanger repairs, and substructure repairs. Special construction sequencing details were developed for 2,700 linear feet of sidewalk replacement to remediate a potential unsafe condition in the adjacent existing deficient barrier construction.
- **MDOT Construction Engineer Taylor TSC: Bill Erben, PE**
  - Telephone: 313-375-2409
  - Client: MDOT
  - Project: I-94 from Rouge River to Wyoming Avenue, Wayne County, MI; 2003 to 2004
    - Site: Depressed Urban Interstate Reconstruction
    - This Southeast Michigan ASCE award-winner, three (3) miles, **4R depressed urban freeway reconstruction** project provided for **reconstruction of the freeway, four interchanges, 13 interchange ramps, and** the rehabilitation of 17 bridges. **Complex items were the curved ramp from US-12 to eastbound I-94, global stability concerns, railroad bridges with deficient vertical clearances,** and complex drainage issues. Major design considerations and innovations included (1) reconfiguration and replacement of all drainage with provisions for inline storage to handle current design requirements and to disconnect from CSO system, (2) **improved freeway and ramp geometry where project constraints permitted,** and (3) **improved underclearances to meet the design guidelines.**
  - Project: I-75 NB & SB over Northline Road and I-75 SB over US-24 Connector, Wayne County, MI; 2016-2017
    - Site: Urban Interstate over Local Road/Ramp
    - For each of the three (3) structures (3 & 4 span steel bridges) involved with this project, the scope of work included: bridge widening, deck & backwall replacement, pier strengthening and cap replacement, structural steel repairs, substructure repairs, approaches and placing EPS blocks behind the existing abutments to upgrade to current AASHTO standards.

- **City of Farmington Hills Traffic Engineer, Mark Saksewski, PE**
  - Telephone: 248-871-2554
  - Client: City of Farmington Hills
  - Projects:
    - Sites: 14 Mile Road from Haggerty to Drake, and Halsted Road from 12 to 14 Mile Roads.
    - Rehabilitation and widening including HMA mill and overlay.
- **Executive Director: Jack Sutton**
  - Telephone: 513-728-3551
  - Client: Great Parks of Hamilton County
  - Project: Lunken-Armleder Trail and Bridge
    - Site: Underdeveloped parkland, that is in the flood plain.
    - The design of a 5,000 foot long ADA compliant trail that connects the Lunken Loop trail to the Armleder Park Trail system. The trail starts in the northeast corner of the Lunken airport property, runs along the top of the flood control levee, drops down to the toe of the levee, passes under the Beechmont Avenue Bridge over the Little Miami River, crosses over the Duck Creek on a 150' long truss bridge and connects to the Armleder Trail system. The truss bridge is designed to be submerged by more than 15' during the Ohio River Flood event. The project also utilized a mechanically stabilized earth retaining wall. The wall exposure is a maximum of 20' high. To meet State and National Scenic River concerns, the face of the wall is a stainless steel basket, with topsoil. The wall was fully planted and is not visible from the river.
  - Other Projects: Oasis Trail, LMST - Newtown Extension; LMST - Columbia TwP Connection
- **Project Manager: Melissa McVay**
  - Telephone: 513-352-5269
  - Client: Cincinnati Department of Transportation and Engineering
  - Project: Little Miami Scenic Trail – Beechmont Avenue
    - Site: Underdeveloped park areas, and archeologically sensitive Native American cultural areas
    - Environmental, planning and design services for the 4000' long “missing link” of the Little Miami Scenic Trail. When complete, users will be able to ride from downtown Cincinnati to downtown Columbus on a continuous asphalt path. IBI Group reviewed the prior trail studies and proposed widening an existing bridge instead of building a new pedestrian bridge. This approach is more ecologically sensitive; meets the goals of the State and National Scenic Rivers; and saved the Owner \$1,000,000. IBI Group also prepared the grant application that secured \$5,200,000 in Federal funds to build the project.
  - Other Project(s): Mill Creek Trail Phases 1-4
- **Senior Planner: Steve Schuckman**
  - Telephone: 513-475-9600
  - Client: Cincinnati Park Board
  - Project: Armleder Park
    - Site: Undeveloped land within floodplain
    - All civil engineering services for this 300-acre park being built by the Cincinnati Recreation Commission in conjunction with the Hamilton County Park District on Park Board property. The park features passive and active recreation areas. The active features include seven soccer fields, miles of multi-use paths, canoe access to the Little Miami River and a dog park. The passive area includes nature trails, meadows and other replanting areas. IBI prepared all civil engineering documents, including: roadway, shared use-path design, storm water, sanitary, water main, irrigation well and grading design for the entire park.
  - Other projects: Washington Park, Lytle Park, Cincinnati Zoo and Botanical Garden



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## Part - II

## Section - A

### Cost Proposal



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
Stony Creek Metropark

Stoney Creek Bridge over 26 Mile, Design Fee					
Alternative 1: Bridge Rehab w/ Widening with precast beams for SUP					
Alternative 2: Bridge Rehab w/ New prefabrictaed steel Ped Bridge for SUP			Alt2, Bridge		
Alternative 3: Bridge Rehab Maintaining existing width and cross section			Alt 1, Bridge	Rehab and new	Alt 3, 4 Bridge Rehab
Alternative 4: Bridge Rehab Maintaining existing width reworking cross section to accommodate SUP			Rehab and Widening	Ped Bridge	Only, No New Ped Bridge
Sheet	Alt 1	Alt 2	Alt 3	Alt 4	
Cover Sheet	1	1	1	1	
Project Info Sheet	1	1	1	1	
Road Sheets with Guard Rail	2	2	2	2	
Path Sheets	1	1	1	1	
MOT Sheets	2	2	1	1	
Pav't Marking & Signing Sheets	1	1	1	1	
General Plan of Structure/Site Plan & Profile	1	1			
General Plan of Structure Sections (Prefab Bridge)	1	1			
Existing General Plan of Site (For Information)	1	1	1	1	
Existing General Plan of Structure ( Proposed Work)	1	1	1	1	
Existing General Plan of Structure (Showing Removal)	1	1	1	1	
Bridge Railing/Fence Removal Sheet	1	1	1	1	
Construction Staging	1	1	1	1	
Deep Foundation Plan and Details	1	1			
Substructure Widening Plan & Elevation 1	1				
Substructure Widening Plan & Elevation 2	1				
Substructure Widening Sections and Details	1				
Concrete Beams Plan	1				
Concrete Beams Sections and Details	1				
Diaphragm Details sheet			1	1	
Deck (Overlay) Plan	1	1	1	1	
Deck (Overlay) Sections and Details	1	1	1	1	
Long Joint Replacement Details	1	1	1	1	
Railing/Fence Details sheet	1	1	1	1	
Concrete Approach Slab Plan (part of road)	0	0	0	0	
Concrete Approach Slab Details (part of Road)	0	0	0	0	
Substructure/Beam/Misc Conc Repair Details	1	1	1	1	
Reinf Sheet	2	2	1	1	
Ped Bridge Site Plan & Profile (Combined with Ex. bridge)		0			
Ped Bridge Substructure Plan and Elevation 1		1			
Ped Bridge Substructure Plan and Elevation 2		1			
Ped Bridge Substructure Details		1			
Ped Bridge Superstructure Plan and Notes		0			
Ped Bridge Approach Plan & Details with road		0			
Total Number of Sheets		28	23	19	19
Design Fee	\$70,000	\$57,500	\$47,500	\$47,500	
Geotech	\$15,000	\$15,000	\$0	\$0	
Design Survey	\$8,000	\$8,000	\$5,000	\$5,000	
Project Development	\$10,000	\$10,000	\$10,000	\$10,000	
Bidding Assistance	\$1,500	\$1,500	\$1,500	\$1,500	
Total	\$104,500	\$92,000	\$64,000	\$64,000	
Alternative 1: Bridge Rehab w/ Widening for SUP					
Alternative 2: Bridge Rehab w/ New Ped Bridge for SUP			Alt2, Bridge		
Alternative 3: Bridge Rehab Maintaining existing width and cross section			Alt 1, Bridge	Rehab and new	Alt 3, 4 Bridge Rehab
Alternative 4: Bridge Rehab Maintaining existing width reworking cross section to accommodate SUP			Rehab and Widening	Ped Bridge	Only, No New Ped Bridge

Billing Schedule				
Project Development	\$10,000	\$10,000	\$10,000	\$10,000
50%	\$35,000	\$28,750	\$23,750	\$23,750
90%	\$28,000	\$23,000	\$19,000	\$19,000
100%	\$7,000	\$5,750	\$4,750	\$4,750
Design Survey, Due at 50% Submittal	\$8,000	\$8,000	\$5,000	\$5,000
Geotech, Due at 50% Submittal	\$15,000	\$15,000	\$0	\$0
Bidding Assistance	\$1,500	\$1,500	\$1,500	\$1,500
Total	\$104,500	\$92,000	\$64,000	\$64,000



# HURON-CLINTON metroparks

Administrative Office | 13000 High Ridge Drive | Brighton, MI 48114-9058  
1-800-477-2757 • www.metroparks.com

## HURON CLINTON METROPOLITAN AUTHORITY REQUEST FOR PROPOSAL STANDARD COST FORM

(TO BE SUBMITTED WITH PROPOSAL PART II)

PROJECT TITLE: 26 Mile Road Bridge Rehabilitation and Pedestrian Bridge Design

PARK NAME: Stony Creek Metropark

RFP No.: P-509-18-550

DUE DATE: May 22, 2018

PROJECT PHASE	ESTIMATED HOURS	ESTIMATED COST
Alternate 1		
Site Investigation, Data Collection, Records Research	included	below
- Geotechnical Investigation	--	15,000.00
- Design survey	--	8,000.00
Project Development		10,000.00
50% Design		35,000.00
90% Design		28,000.00
100% Design		7,000.00
Miscellaneous	--	--
- Meetings	4 Meetings	included
- Permit Preparation and Applications		included
Bidding Assistance/Construction Administration (Allowance)	12 Hrs	1,500.00
Additional Phases/Task As Proposed		
- Construction phase survey and layout		
-		
-		
-		
-		
-		

Include estimated hours for all work performed  
except provided by subconsultant as lump sum.

Proposal Cost (Not to Exceed)

\$104,500.00





# HURON-CLINTON metroparks

Administrative Office | 13000 High Ridge Drive | Brighton, MI 48114-9058  
1-800-477-2757 • www.metroparks.com

## HURON CLINTON METROPOLITAN AUTHORITY REQUEST FOR PROPOSAL STANDARD COST FORM

(TO BE SUBMITTED WITH PROPOSAL PART II)

PROJECT TITLE: 26 Mile Road Bridge Rehabilitation and Pedestrian Bridge Design

PARK NAME: Stony Creek Metropark

RFP No.: P-509-18-550

DUE DATE: May 22, 2018

PROJECT PHASE	ESTIMATED HOURS	ESTIMATED COST
Site Investigation, Data Collection, Records Research		<i>included below</i>
- Geotechnical Investigation	--	<i>15,000.00</i>
- Design survey	--	<i>8,000.00</i>
Project Development		<i>10,000.00</i>
50% Design		<i>28,750.00</i>
90% Design		<i>23,000.00</i>
100% Design		<i>5,750.00</i>
Miscellaneous	--	--
- Meetings	4 Meetings	<i>included</i>
- Permit Preparation and Applications		<i>included</i>
Bidding Assistance/Construction Administration (Allowance)	12 Hrs	<i>1,500.00</i>
Additional Phases/Task As Proposed		
- Construction phase survey and layout		
-		
-		
-		
-		
-		

Include estimated hours for all work performed  
except provided by subconsultant as lump sum.

Proposal Cost (Not to Exceed)

*\$92,000.00*

RFP Standard Cost Form

PART 2 SECTION A



# HURON-CLINTON metroparks

Administrative Office | 13000 High Ridge Drive | Brighton, MI 48114-9058  
1-800-477-2757 • www.metroparks.com

## HURON CLINTON METROPOLITAN AUTHORITY REQUEST FOR PROPOSAL STANDARD COST FORM

(TO BE SUBMITTED WITH PROPOSAL PART II)

PROJECT TITLE: 26 Mile Road Bridge Rehabilitation and Pedestrian Bridge Design

PARK NAME: Stony Creek Metropark

RFP No.: P-509-18-550

DUE DATE: May 22, 2018

PROJECT PHASE	ESTIMATED HOURS	ESTIMATED COST
Site Investigation, Data Collection, Records Research		<i>included below</i>
- Geotechnical Investigation	--	<i>0</i>
- Design survey	--	<i>5,000.00</i>
Project Development		<i>10,000.00</i>
50% Design		<i>23,750.00</i>
90% Design		<i>19,000.00</i>
100% Design		<i>4,750.00</i>
Miscellaneous	--	--
- Meetings	4 Meetings	<i>included</i>
- Permit Preparation and Applications		<i>included</i>
Bidding Assistance/Construction Administration (Allowance)	12 Hrs	<i>1,500.00</i>
Additional Phases/Task As Proposed		
- Construction phase survey and layout		
-		
-		
-		
-		
-		

Include estimated hours for all work performed except provided by subconsultant as lump sum.

Proposal Cost (Not to Exceed)

*\$64,000.00*

RFP Standard Cost Form



# HURON-CLINTON metroparks

Administrative Office | 13000 High Ridge Drive | Brighton, MI 48114-9058  
1-800-477-2757 • www.metroparks.com

## HURON CLINTON METROPOLITAN AUTHORITY REQUEST FOR PROPOSAL STANDARD COST FORM

(TO BE SUBMITTED WITH PROPOSAL PART II)

PROJECT TITLE: 26 Mile Road Bridge Rehabilitation and Pedestrian Bridge Design

PARK NAME: Stony Creek Metropark

RFP No.: P-509-18-550

DUE DATE: May 22, 2018

PROJECT PHASE	ESTIMATED HOURS	ESTIMATED COST
Site Investigation, Data Collection, Records Research		included below
- Geotechnical Investigation	--	Ø
- Design survey	--	5,000.00
Project Development		10,000.00
50% Design		23,750.00
90% Design		19,000.00
100% Design		4,750.00
Miscellaneous	--	--
- Meetings	4 Meetings	included
- Permit Preparation and Applications		included
Bidding Assistance/Construction Administration (Allowance)	12 Hrs	1,500.00
Additional Phases/Task As Proposed		
- Construction phase survey and layout		
-		
-		
-		
-		
-		

Include estimated hours for all work performed  
except provided by subconsultant as lump sum.

Proposal Cost (Not to Exceed)

\$64,000.00

RFP Standard Cost Form

PART 2 SECTION A



IBI GROUP MICHIGAN, LLC  
25200 Telegraph Road – Suite 300  
Southfield MI 48033 USA  
tel 248 936 8000 fax 248 936 8111  
ibigroup.com

## Employee Classifications and Invoicing Rates

### *Classifications*

### *Invoicing Rates*

#### Management Services

Project Manager 1	\$ 135.00
Project Manager 2	\$ 155.00
Senior Professional	\$ 160.00
Project Director	\$ 198.00

#### Strategic & Technical Services

Architect 1	\$ 69.00
Architect 2	\$ 89.00
Architect 3	\$ 118.00
Engineer 1	\$ 76.00
Engineer 2	\$ 98.00
Engineer 3	\$ 128.00
Designer Tech 1	\$ 70.00
Designer Tech 2	\$ 94.00
Designer Tech 3	\$ 115.00
Technical Specialist	\$ 158.00

#### Project Support Staff

Administrative Assistant 1	\$ 58.00
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*Rates are valid through December 31, 2018.*









# Insurance



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
**Stony Creek Metropark**



# CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY)  
05/22/2018

HIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELONGING TO THE CERTIFICATE OF INSURANCE. THIS CERTIFICATE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

**IMPORTANT:** If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

<b>DUCER</b>  n Risk Services Central, Inc. neapolis MN Office 00 West 83rd Street 00 Tower, Suite 1100 neapolis MN 55437 USA	<b>CONTACT NAME:</b>	
	<b>PHONE (A/C, No, EXT):</b>	<b>FAX (A/C, No):</b>
<b>RED</b>  roup Michigan LLC 0 Telegraph Rd, Suite 300 field, MI 48033	<b>E-MAIL ADDRESS:</b>	
	<b>INSURER(S) AFFORDING COVERAGE</b>	
	<b>INSURER A:</b> Liberty Mutual Insurance Company	
	<b>INSURER B:</b>	
	<b>INSURER C:</b> Beazley Insurance Company, Inc.	
	<b>INSURER D:</b>	
<b>INSURER E:</b>		
<b>INSURER F:</b>		

**COVERAGES** **CERTIFICATE NUMBER:** USIBI-1178-1819 **REVISION NUMBER:**

THIS IS TO CERTIFY THAT POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. WITHOUTSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

TYPE OF INSURANCE	ADDL INSRD	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
<input checked="" type="checkbox"/> <b>COMMERCIAL GENERAL LIABILITY</b> <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR  GEN'L AGGREGATE LIMIT APPLIES PER: <input checked="" type="checkbox"/> POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC <input type="checkbox"/> OTHER			TB1-B71-171213-028	04/30/2018	04/30/2019	EACH OCCURRENCE \$ 1,000,000 USD DAMAGE TO RENTED PREMISES (Ea occurrence) \$ 1,000,000 USD MED EXP (Any one person) \$ 2,500 USD PERSONAL & ADV INJURY \$ 1,000,000 USD GENERAL AGGREGATE \$ 1,000,000 USD PRODUCTS - COMP/OP AGG \$ 1,000,000 USD
<b>AUTOMOBILE LIABILITY</b> <input checked="" type="checkbox"/> ANY AUTO <input type="checkbox"/> OWNED AUTOS ONLY <input type="checkbox"/> SCHEDULED AUTOS <input type="checkbox"/> HIRED AUTOS ONLY <input type="checkbox"/> NON-OWNED AUTOS ONLY			AS1-B71-171213-018	04/30/2018	04/30/2019	COMBINED SINGLE LIMIT (Ea accident) \$ 1,000,000 USD BODILY INJURY (Per person) BODILY INJURY (Per accident) PROPERTY DAMAGE (Per accident) \$
<b>UMBRELLA LIAB</b> <input type="checkbox"/> OCCUR <b>EXCESS LIAB</b> <input type="checkbox"/> CLAIMS-MADE <input type="checkbox"/> DED <input type="checkbox"/> RETENTION \$						EACH OCCURRENCE AGGREGATE \$
<b>WORKERS COMPENSATION AND EMPLOYERS' LIABILITY</b> ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? <input type="checkbox"/> Y <input checked="" type="checkbox"/> N (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below		N/A				PER STATUTE <input type="checkbox"/> OTH-ER <input type="checkbox"/> E.L. EACH ACCIDENT \$ E.L. DISEASE - EA EMPLOYEE \$ E.L. DISEASE - POLICY LIMIT \$
Professional Liability			V1F72F170101	04/30/2018	04/30/2019	Per Claim \$ 1,000,000 USD Annual Aggregate \$ 1,000,000 USD

**DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)**  
politan Authority RFP  
with respect to the above and arising out of the Named Insured's operations are the following name(s) added to the policy as Additional Insured(s). The policy limits are not increased by the addition of such Additional Insured as stated in this Certificate  
all, all elected and appointed officials, all employees and volunteers, all boards, commissions, and/or authorities and board members, including employees and officers thereof, where required by written contract or written agreement with respect to Commercial General Liability and Automobile Liability  
Commercial General Liability Policy Includes: Contractual Liability, Products and Completed Operations, Contractor Coverage, Broad form Property Coverage, and Non-Owned Automobile Liability.  
The policy of subrogation applies in favour of the Certificate Holder with respect to the Commercial General Liability policy.

<b>CERTIFICATE HOLDER</b>  n-Clinton Metropolitan Authority 0 High Ridge Drive nton, MI 48114  Engineering Department	<b>CANCELLATION</b>  SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.  <b>AUTHORIZED REPRESENTATIVE</b>  Andrea R. Otto 
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Policy Number TB1-B71-171213-028  
Issued By Liberty Mutual Insurance Company

**THIS ENDORSEMENT CHANGES THE POLICY. PLEASE READ IT CAREFULLY.**

**NOTICE OF CANCELLATION TO THIRD PARTIES**

- A. If we cancel this policy for any reason other than non-payment of premium, we will provide 30 days notification (except 15 days for non-payment of premium) to the persons or organizations shown in the Schedule below. In no event does the notice to the third party exceed the notice to the first named insured.
- B. This notification of a pending cancellation of coverage is intended as a courtesy only. Our failure to provide such advance notification will not extend the policy cancellation date nor negate cancellation of the policy.

SCHEDULE
All certificate holders where written notice of the cancellation of this policy is required by written contract, permit or agreement with the Named Insured or those entities the Named Insured has responsibility to place this insurance for, and whose names and addresses will be provided by the broker or agent listed in the Declarations Page of this policy for the purposes of complying with such request.

All other terms and conditions of this policy remain unchanged.

Policy Number TB1-B71-171213-028  
Issued by LIBERTY MUTUAL INSURANCE COMPANY

**THIS ENDORSEMENT CHANGES THE POLICY. PLEASE READ IT CAREFULLY.**

**BLANKET ADDITIONAL INSURED**

This endorsement modifies insurance provided under the following:

COMMERCIAL GENERAL LIABILITY COVERAGE FORM

SECTION II - WHO IS AN INSURED is amended to include as an insured any person or organization for whom you have agreed in writing to provide liability insurance. But:

The insurance provided by this amendment:

1. Applies only to "bodily injury" or "property damage" arising out of (a) "your work" or (b) premises or other property owned by or rented to you;
2. Applies only to coverage and minimum limits of insurance required by the written agreement, but in no event exceeds either the scope of coverage or the limits of insurance provided by this policy; and
3. Does not apply to any person or organization for whom you have procured separate liability insurance while such insurance is in effect, regardless of whether the scope of coverage or limits of insurance of this policy exceed those of such other insurance or whether such other insurance is valid and collectible.

The following provisions also apply:

1. Where the applicable written agreement requires the insured to provide liability insurance on a primary, excess, contingent, or any other basis, this policy will apply solely on the basis required by such written agreement and Item 4. Other Insurance of SECTION IV of this policy will not apply.
2. Where the applicable written agreement does not specify on what basis the liability insurance will apply, the provisions of Item 4. Other Insurance of SECTION IV of this policy will govern.
3. This endorsement shall not apply to any person or organization for any "bodily injury" or "property damage" if any other additional insured endorsement on this policy applies to that person or organization with regard to the "bodily injury" or "property damage".
4. If any other additional insured endorsement applies to any person or organization and you are obligated under a written agreement to provide liability insurance on a primary, excess, contingent, or any other basis for that additional insured, this policy will apply solely on the basis required by such written agreement and Item 4. Other Insurance of SECTION IV of this policy will not apply, regardless of whether the person or organization has available other valid and collectible insurance. If the applicable written agreement does not specify on what basis the liability insurance will apply, the provisions of Item 4. Other Insurance of SECTION IV of this policy will govern.

# WAIVER OF TRANSFER OF RIGHTS OF RECOVERY AGAINST OTHERS TO US

This endorsement modifies insurance provided under the following:

COMMERCIAL GENERAL LIABILITY COVERAGE PART  
PRODUCTS/COMPLETED OPERATIONS LIABILITY COVERAGE PART

## SCHEDULE

<b>Name Of Person Or Organization:</b>
--

As required by written contract or agreement entered into prior to loss
---

Information required to complete this Schedule, if not shown above, will be shown in the Declarations.
--

The following is added to Paragraph **8. Transfer Of Rights Of Recovery Against Others To Us** of **Section IV – Conditions:**

We waive any right of recovery we may have against the person or organization shown in the Schedule above because of payments we make for injury or damage arising out of your ongoing operations or "your work" done under a contract with that person or organization and included in the "products-completed operations hazard". This waiver applies only to the person or organization shown in the Schedule above.



COMMERCIAL AUTO  
CA 20 48 10 13

**DESIGNATED INSURED FOR  
COVERED AUTOS LIABILITY COVERAGE**

AUTO DEALERS COVERAGE FORM  
BUSINESS AUTO COVERAGE FORM  
MOTOR CARRIER COVERAGE FORM

This endorsement identifies person(s) or organization(s) who are "insureds" for Covered Autos Liability Coverage under the Who Is An Insured provision of the Coverage Form. This endorsement does not alter coverage provided in the Coverage Form.

Name Of Person(s) Or Organization(s):
Refer to Designated Insured Schedule
Information required to complete this Schedule, if not shown above, will be shown in the Declarations.

138 of 156







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# RFP



26 Mile Road Bridge Rehabilitation And Pedestrian Bridge Design  
**Stony Creek Metropark**









## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
From: Amy McMillan, Director  
Subject: Board of Commission – Officer Elections  
Date: June 7, 2018

**Requested Action: Motion to Elect Board Officers**

That the Board of Commissioners' elect a Chairman, Vice-Chairman, Treasurer and Secretary at the June 8, 2017 Board meeting as required by the Metroparks Bylaws.

**Background:** The Huron-Clinton Metropolitan Authority Bylaws read in part:

*The Board of Commissioners shall elect a Chairman, a Vice-Chairman, a Treasurer and a Secretary, who shall be elected by the Commissioners at the annual meeting of the Board of Commissioners.*

The regular monthly meeting held in the month of June constitutes the annual meeting of the Board of Commissioners.



## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
From: Nina Kelly, Manager of Planning  
Subject: E-Bike Presentation and Demonstration  
Date: June 7, 2018

### Action Requested: Motion to Receive and File

That the Board of Commissioners' receive and file a presentation and demonstration of Class 1 and Class 2 e-bikes by Harrison Township business owner and e-bike seller, Greg Krzeminski, as recommended by Manager of Planning Nina Kelly and staff.

**Background:** Three bills regarding electric bicycles, or e-bikes, passed legislation Oct. 29, 2017. House Bills 4781, 4782 and 4783, make amendments to the Michigan Vehicle Code (MVC) to establish three definitions and classifications for e-bikes along with an outline of conditions for lawfully operating them on trails and roadways.

Class 1 e-bike: equipped with an electric motor that provides assistance only when the rider is pedalling and that stops providing assistance when the e-bike reaches a speed of 20 miles per hour.

Class 2 e-bike: equipped with a motor that propels the electric bicycles to a speed of no more than 20 miles per hour whether the rider is pedalling or not and that disengages or ceases to function when the brakes are applied.

Class 3 e-bike: equipped with a motor that provides assistance only when the rider is pedalling and stops providing that assistance when the electric bicycle reaches a speed of 28 miles per hour.

Class 1 e-bikes are now automatically able to be operated on paved trails in the Metroparks. Some neighboring communities are considering "opting in" to also allow Class 2 e-bikes on paved trails.



## HURON-CLINTON METROPOLITAN AUTHORITY

To: Board of Commissioners  
From: Amy McMillan, Director  
Subject: Report – Memorial Day Weekend  
Date: June 7, 2018

**Action Requested: Motion to Receive and File**

That the Board of Commissioners' receive and file Memorial Day Weekend report as recommended by Director Amy McMillan and staff.

**Background:** The Memorial Day weekend, which is the unofficial start of the summer season, was sunny, hot and the Metroparks were filled with patrons.

Kensington, Lake St. Clair, Stony Creek and Lower Huron had record breaking revenue with a three-day total of more than \$50,000 at each park.

**Attachment: Memorial Day Weekend by the Numbers**

**EASTERN DISTRICT  
MEMORIAL DAY WEEKEND**

**Stony Creek**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold	Golf Rounds	Golf Revenue	Tolling Revenue	Other Revenue	Total Revenue
Saturday, May 26, 2018	AM Rain 85	2,940	608	417	1,025	165	\$7,682	\$20,228	\$5,965	\$33,875
Sunday, May 27, 2018	Sunny 90	5,804	2,005	1,020	3,025	163	\$7,912	\$54,701	\$11,711	\$74,324
Monday, May 28, 2018	Sunny 92	8,342	4,224	1,356	5,580	130	\$5,554	\$87,500	\$15,450	\$108,504
Saturday, May 27, 2017	Sunny 75	3,989	947	688	1,635	204	\$8,751	\$32,557	\$6,427	\$47,735
Sunday, May 28, 2017	PM Rain 76	3,438	720	447	1,167	127	\$5,352	\$22,262	\$3,790	\$31,404
Monday, May 29, 2017	PM Rain 79	7,001	3,155	952	4,107	155	\$6,663	\$69,799	\$11,020	\$87,482
Total 2018		17,086	6,837	2,793	9,630	458	\$21,148	\$162,429	\$17,676	\$216,703
Total 2017		14,428	6,229	2,087	6,909	486	\$20,766	\$124,618	\$10,217	\$166,621
Difference		2,658	608	706	2,721	(28)	\$382	\$37,811	\$7,459	\$50,082

**Lake St. Clair**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold	Golf Rounds	Golf Revenue	Tolling Revenue	Other Revenue	Total Revenue
Saturday, May 26, 2018	AM Rain 85	2,276	582	296	878	43	\$354	\$15,101	\$3,079	\$18,534
Sunday, May 27, 2018	Sunny 90	4,381	1,597	794	2,391	60	\$547	\$41,503	\$7,897	\$49,947
Monday, May 28, 2018	Sunny 92	7,211	4,302	910	5,212	45	\$362	\$72,157	\$15,217	\$87,736
Saturday, May 27, 2017	Sunny 75	2,787	744	385	1,129	60	\$448	\$20,147	\$2,558	\$23,153
Sunday, May 28, 2017	PM Rain 76	2,119	538	268	806	78	\$685	\$13,690	\$1,387	\$15,763
Monday, May 29, 2017	PM Rain 78	4,581	2,177	452	2,629	67	\$476	\$42,591	\$6,507	\$49,574
Total 2018		13,868	6,481	2,000	8,481	148	\$1,263	\$128,761	\$26,193	\$156,217
Total 2017		9,487	3,459	1,105	4,564	205	\$1,609	\$76,428	\$10,452	\$88,489
Difference		4,381	3,022	895	3,917	(57)	(\$346)	\$52,333	\$15,741	\$67,728



**SOUTHERN DISTRICT  
MEMORIAL DAY WEEKEND**

**Lower Huron, Willow & Oakwoods Metroparks**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold
Saturday, May 26, 2018	80 cloudy/rain	1,964	281	155	436
Sunday, May 27, 2018	95 sunny	3,386	1,370	356	1,726
Monday, May 28, 2018	93 sunny	6,048	1,855	398	2,253
Saturday, May 27, 2017	75 Sunny	2,337	387	174	561
Sunday, May 28, 2017	74 Sun/Rain	2,021	331	108	439
Monday, May 29, 2017	80 Sun/Rain	3,068	1,164	339	1,503
Total 2018		11,398	3,506	909	4,415
Total 2017		7,426	1,882	621	2,503
Difference		3,972	1,624	288	1,912

Dates	Golf Rounds	Golf Revenue	Turtle Cove Swimmers	Turtle Cove Revenue	Willow Pool Swimmers	Willow Pool Revenue	Tolling Revenue	Other Revenue	Total Revenue
Saturday, May 26, 2018	0	\$97	249	\$2,787	53	\$217	\$7,619	\$1,111	\$11,831
Sunday, May 27, 2018	0	\$57	2559	\$26,153	390	\$2,030	\$24,744	\$507	\$53,491
Monday, May 28, 2018	114	\$3,098	2854	\$30,398	1,030	\$5,199	\$31,472	\$557	\$70,724
Saturday, May 27, 2017	132	\$5,161	235	\$2,302	63	\$417	\$9,080	\$998	\$17,958
Sunday, May 28, 2017	100	\$4,286	134	\$1,579	37	\$276	\$6,754	\$331	\$13,226
Monday, May 29, 2017	169	\$6,690	1387	\$14,235	335	\$1,823	\$22,679	\$552	\$45,979
Total 2018	0	\$154	5662	\$59,338	1473	\$7,446	\$32,363	\$1,618	\$136,046
Total 2017	401	\$16,137	1756	\$18,116	435	\$2,516	\$38,513	\$1,881	\$77,163
Difference	(401)	(\$15,983)	\$3,906	\$41,222	\$1,038	\$4,930	(\$6,150)	(\$263)	\$58,883

**SOUTHERN DISTRICT  
MEMORIAL DAY WEEKEND**

**Lake Erie**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold	Golf Rounds	Golf Revenue	Tolling Revenue	Other Revenue	Total Revenue
Saturday, May 26, 2018	Rain 75	878	84	99	183	125	\$3,658	\$4,748	\$3,651	\$12,057
Sunday, May 27, 2018	AM Fog/ PM Sun 79	1,660	482	191	673	125	\$4,190	\$11,442	\$8,183	\$23,816
Monday, May 28, 2018	Sunny 82	2,169	829	254	1,083	124	\$4,219	\$17,019	\$16,014	\$37,252
Saturday, May 27, 2017	Rain 68	1308	169	147	316	124	\$5,268	\$7,017	\$2,644	\$14,928
Sunday, May 28, 2017	Rain 71	859	118	74	192	90	\$3,812	\$3,777	\$1,953	\$9,543
Monday, May 29, 2017	Rain 79	1623	369	117	486	160	\$7,540	\$9,668	\$5,472	\$22,680
Total 2018		4,707	1,395	544	1,939	250	\$12,067	\$33,209	\$27,848	\$73,125
Total 2017		3,790	656	338	994	214	\$16,620	\$20,462	\$10,069	\$47,151
Difference		917	739	206	945	36	(\$4,553)	\$12,747	\$17,779	\$25,974

**WESTERN DISTRICT  
MEMORIAL DAY WEEKEND**

**Kensington**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold	Golf Rounds	Golf Revenue	Tolling Revenue	Other Revenue	Farm Grille Revenue	Total Revenue
Saturday, May 26, 2018	81 Cloudy	4,483	873	677	1,550	175	\$7,370	\$29,831	\$6,275	\$872	\$44,348
Sunday, May 27, 2018	89 Sunny	6,788	2,419	1,224	3,643	235	\$9,994	\$63,692	\$18,110	\$1,229	\$93,024
Monday, May 28, 2018	92 Sunny	9,026	4,377	1,269	5,646	142	\$6,090	\$84,891	\$21,134	\$1,100	\$113,216
Saturday, May 27, 2017	SUNNY 75	6,294	1,560	964	2,524	184	\$7,693	\$45,987	\$10,310	\$1,390	\$65,379
Sunday, May 28, 2017	OVER/RAIN 75	4,442	929	578	1,507	140	\$5,824	\$28,040	\$5,392	\$1,092	\$40,349
Monday, May 29, 2017	SUNNY 80	7,108	2,843	1,080	3,923	225	\$8,498	\$62,925	\$11,569	\$1,342	\$85,675
Total 2018		20,297	7,669	3,170	10,839	552	\$23,454	\$178,414	\$45,520	\$3,200	\$250,588
Total 2017		17,844	5,332	2,622	7,954	549	\$22,015	\$136,952	\$27,271	\$3,824	\$191,403
Difference		2,453	2,337	548	2,885	3	\$1,439	\$41,462	\$18,248	(\$624)	\$59,185

**Indian Springs**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold	Golf Rounds	Golf Revenue	Tolling Revenue	Other Revenue	Total Revenue
Saturday, May 26, 2018	Sun	492	109	35	157	105	\$ 4,254	\$ 2,926	\$ 1,700	\$ 8,880
Sunday, May 27, 2018	Sun	565	98	50	164	178	\$ 7,023	\$ 3,104	\$ -	\$ 10,085
Monday, May 28, 2018	Sun	470	91	42	148	121	\$ 4,627	\$ 2,798	\$ 2,351	\$ 9,776
Saturday, May 27, 2017	Sun	512	74	25	110	127	\$ 5,193	\$ 1,884	\$ -	\$ 7,077
Sunday, May 28, 2017	Sun/Rain	316	50	24	89	115	\$ 4,891	\$ 1,651	\$ -	\$ 6,542
Monday, May 29, 2017	Sun	743	96	96	165	205	\$ 8,053	\$ 3,235	\$ 42	\$ 11,330
Total 2018		1,527	298	127	469	404	\$ 15,904	\$ 8,828	\$ 4,051	\$ 28,741
Total 2017		1,571	220	145	364	447	\$ 18,137	\$ 6,770	\$ 42	\$ 24,949
Difference		(44)	78	(18)	105	(43)	(\$2,233)	\$ 2,058	\$ 4,009	\$ 3,792

**WESTERN DISTRICT  
MEMORIAL DAY WEEKEND**

**Huron Meadows**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold	Golf Rounds	Golf Revenue	Tolling Revenue	Other Revenue	Total Revenue
Saturday, May 26, 2018	Mostly Sunny 83	388	0	3	3	133	\$5,225	\$77	\$0	\$5,225
Sunday, May 27, 2018	Sunny & Hot 91	412	5	7	12	185	\$7,388	\$147	\$0	\$7,388
Monday, May 28, 2018	Sunny, Hot & Humid 90	508	4	10	14	139	\$5,740	\$136	\$0	\$5,740
Saturday, May 27, 2017	P. Sunny 74	497	19	10	29	204	\$7,871	\$547	\$0	\$8,418
Sunday, May 28, 2017	Cloudy, Humid some rain 77	378	7	10	17	103	\$4,125	\$392	\$0	\$4,517
Monday, May 29, 2017	Mostly Sunny, scattered rain 77	569	14	19	33	185	\$7,971	\$805	\$0	\$8,776
Total 2018		1,308	9	20	29	457	18,353	360	0	18,353
Total 2017		1,444	40	39	79	492	19,967	1,744	0	21,711
Difference		(136)	(31)	(19)	(50)	(35)	(\$1,614)	(\$1,384)	\$0	(\$3,358)

**Hudson Mills, Dexter-Huron & Delhi**

Dates	Weather	Vehicle Entries	Daily Permits	Annual Permits	Total Permits Sold	Golf Rounds	Golf Revenue	Tolling Revenue	Other Revenue	Total Revenue
Saturday, May 26, 2018	Mostly Sunny 83	694	49	68	117	74	\$2,744	\$2,646	\$276	\$5,666
Sunday, May 27, 2018	Sunny & Hot 91	1,313	209	164	373	65	\$2,586	\$7,564	\$268	\$10,418
Monday, May 28, 2018	Sunny/Hot/Humid 93	1,348	319	172	491	66	\$2,549	\$8,250	\$503	\$11,302
Saturday, May 27, 2017	P. Sunny 74	1,165	171	113	284	106	\$3,438	\$5,371	\$415	\$9,224
Sunday, May 28, 2017	Cloudy/Humid scat rain 77	916	83	74	157	71	\$2,636	\$3,224	\$469	\$6,329
Monday, May 29, 2017	Misty Sun/scat rain 77	1,631	268	193	461	88	\$2,793	\$8,253	\$625	\$11,671
Total 2018		3,355	577	404	981	205	7,879	18,460	1,047	27,386
Total 2017		3,712	522	380	902	265	8,867	16,848	1,509	27,224
Difference		(357)	55	24	79	(60)	(\$988)	\$1,612	(\$462)	\$162



PARK	MONTHLY VEHICLE ENTRIES				MONTHLY TOLL REVENUE			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Lake St Clair	52,473	38,659	43,295	21%	\$ 335,871	\$ 206,513	\$ 234,925	43%
Wolcott Mill	6,357	8,169	6,782	-6%	\$ 4,191	\$ 7,700	\$ 3,011	39%
Stony Creek	73,314	58,992	63,509	15%	\$ 458,550	\$ 319,150	\$ 347,239	32%
Indian Springs	11,498	10,426	11,148	3%	\$ 50,201	\$ 43,667	\$ 45,045	11%
Kensington	89,209	83,850	89,147	0%	\$ 455,929	\$ 372,420	\$ 367,820	24%
Huron Meadows	10,245	9,785	10,006	2%	\$ 3,052	\$ 4,444	\$ 4,493	-32%
Hudson Mills	22,140	21,692	22,691	-2%	\$ 72,701	\$ 71,492	\$ 75,687	-4%
Lower Huron/Willow/Oakwoods	60,629	57,179	58,078	4%	\$ 146,809	\$ 111,985	\$ 125,537	17%
Lake Erie	20,947	19,618	18,638	12%	\$ 89,069	\$ 62,689	\$ 73,848	21%
Monthly TOTALS	346,812	308,370	323,294	7%	\$ 1,616,373	\$ 1,200,060	\$ 1,277,605	27%

PARK	Y-T-D VEHICLE ENTRIES				Y-T-D TOLL REVENUE			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Lake St Clair	128,120	111,490	114,350	12%	\$ 638,101	\$ 492,469	\$ 487,721	31%
Wolcott Mill	21,374	24,953	21,294	0%	\$ 21,587	\$ 19,843	\$ 7,939	172%
Stony Creek	180,285	162,964	161,124	12%	\$ 920,866	\$ 935,458	\$ 876,030	5%
Indian Springs	25,401	26,533	27,075	-6%	\$ 121,977	\$ 138,951	\$ 127,462	-4%
Kensington	260,609	252,702	258,789	1%	\$ 1,086,210	\$ 1,118,054	\$ 991,224	10%
Huron Meadows	29,678	28,307	29,736	0%	\$ 30,896	\$ 21,587	\$ 30,114	3%
Hudson Mills	65,716	67,806	65,916	0%	\$ 194,344	\$ 234,495	\$ 218,419	-11%
Lower Huron/Willow/Oakwoods	190,786	195,269	183,987	4%	\$ 322,315	\$ 340,069	\$ 316,236	2%
Lake Erie	59,157	61,506	57,590	3%	\$ 258,873	\$ 253,937	\$ 245,851	5%
Monthly TOTALS	961,126	931,530	919,860	4%	\$ 3,595,169	\$ 3,554,863	\$ 3,300,996	9%

PARK	MONTHLY PARK REVENUE				Y-T-D PARK REVENUE			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Lake St Clair	\$ 403,872	\$ 241,628	\$ 358,867	13%	\$ 941,652	\$ 762,779	\$ 769,763	22%
Wolcott Mill	\$ 26,162	\$ 30,330	\$ 60,902	-57%	\$ 112,414	\$ 113,114	\$ 143,544	-22%
Stony Creek	\$ 716,869	\$ 531,056	\$ 586,001	22%	\$ 1,384,016	\$ 1,371,269	\$ 1,310,902	6%
Indian Springs	\$ 174,765	\$ 148,372	\$ 157,348	11%	\$ 313,039	\$ 339,748	\$ 326,652	-4%
Kensington	\$ 717,418	\$ 562,378	\$ 590,522	21%	\$ 1,566,438	\$ 1,479,594	\$ 1,426,422	10%
Huron Meadows	\$ 132,171	\$ 115,952	\$ 114,122	16%	\$ 234,601	\$ 205,825	\$ 213,012	10%
Hudson Mills	\$ 160,455	\$ 136,479	\$ 146,022	10%	\$ 343,840	\$ 365,086	\$ 352,204	-2%
Lower Huron/Willow/Oakwoods	\$ 294,459	\$ 233,276	\$ 268,886	10%	\$ 544,285	\$ 522,317	\$ 531,149	2%
Lake Erie	\$ 240,752	\$ 190,220	\$ 215,253	12%	\$ 476,442	\$ 465,449	\$ 467,902	2%
Y-T-D TOTALS	\$ 2,866,924	\$ 2,189,691	\$ 2,497,923	15%	\$ 5,916,727	\$ 5,625,181	\$ 5,541,548	7%

District	Y-T-D Vehicle Entries by Management Unit				Y-T-D Total Revenue by Management Unit			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Eastern	329,779	299,407	296,768	11%	2,438,082	2,247,162	2,224,209	10%
Western	381,404	375,348	381,515	0%	2,457,918	2,390,253	2,318,289	6%
Southern	249,943	256,775	241,577	3%	1,020,727	987,766	999,051	2%

GOLF THIS MONTH	MONTHLY ROUNDS				MONTHLY REVENUE			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Stony Creek	4,907	3,911	4,293	14%	\$ 159,574	\$ 129,622	\$ 141,774	13%
Indian Springs	3,813	3,214	3,330	14%	\$ 110,540	\$ 91,647	\$ 85,547	29%
Kensington	4,967	4,175	4,328	15%	\$ 152,516	\$ 124,251	\$ 116,442	31%
Huron Meadows	4,022	3,393	3,582	12%	\$ 127,720	\$ 110,708	\$ 104,821	22%
Hudson Mills	3,134	2,263	2,449	28%	\$ 75,071	\$ 52,464	\$ 53,049	42%
Willow	1,551	2,673	3,268	-53%	\$ 46,256	\$ 80,156	\$ 88,442	-48%
Lake Erie	3,629	3,292	3,608	1%	\$ 99,636	\$ 90,122	\$ 89,282	12%
Total Regulation	26,023	22,921	24,859	5%	\$ 771,313	\$ 678,970	\$ 679,357	14%
LSC Par 3	861	818	867	-1%	\$ 4,890	\$ 5,130	\$ 5,369	-9%
LSC Foot Golf	8	47	79	-90%	\$ 54	\$ 316	\$ 552	-90%
Total Golf	26,892	23,786	26,300	2%	\$ 776,257	\$ 684,416	\$ 688,671	13%

GOLF Y-T-D	GOLF ROUNDS Y-T-D				GOLF REVENUE Y-T-D			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Stony Creek	6,238	6,098	6,412	-3%	\$ 198,477	\$ 192,288	\$ 200,662	-1%
Indian Springs	5,021	4,974	5,026	0%	\$ 142,975	\$ 138,385	\$ 126,678	13%
Kensington	6,989	6,602	6,680	5%	\$ 206,015	\$ 190,683	\$ 174,038	18%
Huron Meadows	5,754	5,611	5,567	3%	\$ 174,763	\$ 172,498	\$ 159,602	9%
Hudson Mills	4,422	3,319	3,505	26%	\$ 98,842	\$ 76,799	\$ 74,649	32%
Willow	2,800	3,479	4,626	-39%	\$ 78,036	\$ 103,720	\$ 124,929	-38%
Lake Erie	4,854	5,096	5,335	-9%	\$ 132,637	\$ 140,685	\$ 131,515	1%
Total Regulation	36,078	35,179	37,152	-3%	\$ 1,031,745	\$ 1,015,058	\$ 992,073	4%
LSC Par 3	1,112	1,099	1,088	2%	\$ 6,442	\$ 6,876	\$ 8,267	-22%
LSC Foot Golf	19	75	105	-82%	\$ 105	\$ 518	\$ 708	-85%
Total Golf	37,209	36,353	39,042	-5%	\$ 1,038,292	\$ 1,022,452	\$ 1,005,827	3%

AQUATICS THIS MONTH	PATRONS THIS MONTH				MONTHLY REVENUE			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Lake St. Clair	4,878	1,270	2,396	104%	\$ 24,366	\$ 6,420	\$ 11,415	113%
Stony Creek Rip Slide	2,062	1,138	1,412	46%	\$ 8,788	\$ 5,778	\$ 7,153	23%
KMP Splash	4,345	1,302	2,056	111%	\$ 26,172	\$ 7,934	\$ 12,187	115%
Lower Huron	6,637	1,811	3,211	107%	\$ 68,104	\$ 18,719	\$ 30,910	120%
Willow	1,473	435	849	73%	\$ 7,445	\$ 2,570	\$ 4,054	84%
Lake Erie	3,788	699	1,603	136%	\$ 22,813	\$ 6,099	\$ 12,047	89%
TOTALS	23,183	6,655	11,655	99%	\$ 157,688	\$ 47,520	\$ 78,658	100%

AQUATICS Y-T-D	PATRONS Y-T-D				REVENUE Y-T-D			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Lake St. Clair	4,878	1,270	2,396	104%	\$ 24,366	\$ 6,420	\$ 11,415	113%
Stony Creek Rip Slide	2,062	1,138	1,412	46%	\$ 8,788	\$ 5,778	\$ 7,153	23%
KMP Splash	4,345	1,302	2,056	111%	\$ 28,372	\$ 10,334	\$ 14,434	97%
Lower Huron	6,637	1,811	3,211	107%	\$ 68,104	\$ 18,719	\$ 30,910	120%
Willow	1,473	435	849	73%	\$ 7,445	\$ 2,570	\$ 4,054	84%
Lake Erie	3,788	699	1,603	136%	\$ 22,813	\$ 6,099	\$ 12,047	89%
TOTALS	23,183	6,655	11,655	99%	\$ 159,888	\$ 49,920	\$ 80,905	98%

PARK	Seasonal Activities this Month				Monthly Revenue			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
<b>Lake St. Clair</b>								
Welsh Center	4	0	2	71%	\$ 3,800	\$ 800	\$ 2,533	50%
Shelters	46	28	49	-7%	\$ 10,539	\$ 8,700	\$ 12,248	-14%
Boat Launches	275	650	626	-56%	\$ -	\$ -	\$ -	-
Marina	259	426	417	-38%	\$ 5,704	\$ 328	\$ 1,406	306%
Mini-Golf	645	620	640	1%	\$ 2,424	\$ 2,229	\$ 2,196	10%
<b>Wolcott</b>								
Activity Center	14	10	10	40%	\$ 1,000	\$ 1,500	\$ 2,483	-60%
<b>Stony Creek</b>								
Disc Golf Daily	2,305	2,110	2,838	-19%	\$ 6,915	\$ 6,330	\$ 8,871	-22%
Disc Golf Annual	28	17	26	9%	\$ 1,540	\$ 895	\$ 1,233	25%
Total Disc Golf	2,333	2,127	2,864	-19%	\$ 8,455	\$ 7,225	\$ 10,104	-16%
Shelters	81	65	68	20%	\$ 18,225	\$ 14,625	\$ 14,117	29%
Boat Rental	1,964	1,154	1,604	22%	\$ 20,552	\$ 12,539	\$ 16,542	24%
Boat Launches	187	126	120	56%	\$ -	\$ -	\$ -	-
<b>Indian Springs</b>								
Shelters	13	3	9	44%	\$ 2,550	\$ 450	\$ 1,050	143%
Event Room	4	4	5	-14%	\$ 5,600	\$ 7,900	\$ 7,575	-26%
<b>Kensington</b>								
Disc Golf Daily	2,745	3,317	4,396	-38%	\$ 8,235	\$ 9,951	\$ 11,826	-30%
Disc Golf Annual	34	22	24	40%	\$ 1,810	\$ 1,210	\$ 1,199	51%
Total Disc Golf	2,779	3,339	4,420	-37%	\$ 10,045	\$ 11,161	\$ 13,025	-23%
Shelters	72	65	71	1%	\$ 16,200	\$ 14,625	\$ 16,525	-2%
Boat Rental	1,951	1,345	1,653	18%	\$ 26,092	\$ 16,223	\$ 20,643	26%
<b>Huron Meadows</b>								
Shelters	7	4	5	40%	\$ 1,400	\$ 800	\$ 1,200	17%
<b>Hudson Mills</b>								
Disc Golf Daily	846	1,468	1,598	-47%	\$ 2,538	\$ 4,404	\$ 4,795	-47%
Disc Golf Annual	24	21	25	-3%	\$ 1,280	\$ 1,115	\$ 1,270	1%
Total Disc Golf	870	1,489	1,623	-46%	\$ 3,818	\$ 5,519	\$ 6,065	-37%
Shelters	20	21	19	5%	\$ 4,000	\$ 4,200	\$ 3,800	5%
Canoe Rental	0	213	338	-	\$ -	\$ 982	\$ 1,707	-
<b>Lower Huron / Willow / Oakwoods</b>								
Disc Golf Daily	118	229	76	55%	\$ 354	\$ 687	\$ 229	55%
Disc Golf Annual	2	5	2	20%	\$ 110	\$ 275	\$ 92	20%
Total Disc Golf	120	234	78	54%	\$ 464	\$ 962	\$ 321	45%
Shelters	54	49	42	30%	\$ 11,750	\$ 10,600	\$ 8,933	32%
<b>Lake Erie</b>								
Shelters	13	13	14	-5%	\$ 2,700	\$ 2,900	\$ 3,067	-12%
Boat Launches	2,325	2,117	2,089	11%	\$ -	\$ -	\$ -	-
Marina	238	0	2	14180%	\$ 24,093	\$ 26,378	\$ 27,673	-13%

PARK	Seasonal Activities Y-T-D				Seasonal Revenue Y-T-D			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
<b>Lake St. Clair</b>								
Welsh Center	34	23	20	73%	\$ 24,950	\$ 19,200	\$ 16,442	52%
Shelters	144	112	146	-1%	\$ 34,214	\$ 32,750	\$ 36,015	-5%
Boat Launches	438	763	750	-42%	\$ -	\$ -	\$ -	-
Marina	259	427	419	-38%	\$ 5,704	\$ 342	\$ 1,420	302%
Mini-Golf	645	620	640	1%	\$ 2,424	\$ 2,229	\$ 2,196	10%
<b>Wolcott</b>								
Activity Center	39	38	41	-6%	\$ 10,150	\$ 7,000	\$ 8,883	14%
<b>Stony Creek</b>								
Disc Golf Daily	3,860	4,991	5,480	-30%	\$ 12,030	\$ 15,051	\$ 16,824	-28%
Disc Annual	95	117	113	-16%	\$ 5,205	\$ 6,275	\$ 5,723	-9%
Total Disc Golf	3,955	5,108	5,593	-29%	\$ 17,235	\$ 21,326	\$ 22,547	-24%
Shelters	181	191	214	-15%	\$ 40,725	\$ 42,875	\$ 44,237	-8%
Boat Rental	1,964	1,154	1,604	22%	\$ 20,552	\$ 12,539	\$ 16,542	24%
Boat Launches	187	149	186	1%	\$ -	\$ -	\$ -	-
<b>Indian Springs</b>								
Shelters	21	16	25	-15%	\$ 4,050	\$ 3,050	\$ 3,817	6%
Event Room	20	25	21	-5%	\$ 31,400	\$ 45,400	\$ 36,579	-14%
<b>Kensington</b>								
Disc Golf Daily	5,349	7,992	8,027	-33%	\$ 16,047	\$ 23,976	\$ 24,082	-33%
Disc Annual	144	111	141	2%	\$ 7,760	\$ 6,005	\$ 6,914	12%
Total Disc Golf	5,493	8,103	8,169	-33%	\$ 23,807	\$ 29,981	\$ 30,996	-23%
Shelters	240	194	236	2%	\$ 54,000	\$ 51,200	\$ 57,962	-7%
Boat Rental	1,951	1,345	1,653	18%	\$ 26,092	\$ 16,223	\$ 20,643	26%
<b>Huron Meadows</b>								
Shelters	17	11	15	13%	\$ 3,400	\$ 2,200	\$ 3,267	4%
<b>Hudson Mills</b>								
Disc Golf Daily	2,328	4,021	3,751	-38%	\$ 6,984	\$ 12,063	\$ 11,252	-38%
Disc Annual	149	135	128	16%	\$ 7,995	\$ 7,225	\$ 6,625	21%
Total Disc Golf	2,477	4,156	3,879	-36%	\$ 14,979	\$ 19,288	\$ 17,877	-16%
Shelters	60	64	64	-6%	\$ 13,400	\$ 12,800	\$ 12,800	5%
Canoe Rental	0	213	338	-	\$ -	\$ 5,698	\$ 3,279	-
<b>Lower Huron / Willow / Oakwoods</b>								
Disc Golf Daily	501	473	158	218%	\$ 1,503	\$ 1,419	\$ 473	218%
Disc Annual	12	9	3	300%	\$ 640	\$ 495	\$ 165	288%
Total Disc Golf	513	482	161	219%	\$ 2,143	\$ 1,914	\$ 638	236%
Shelters	175	153	153	15%	\$ 38,450	\$ 33,550	\$ 33,392	15%
<b>Lake Erie</b>								
Shelters	33	31	37	-12%	\$ 7,200	\$ 7,000	\$ 8,300	-13%
Boat Launches	6,154	5,884	5,540	11%	\$ -	\$ -	\$ -	-
Marina	352	0	3	13100%	\$ 48,289	\$ 47,661	\$ 50,807	-5%



## INTERPRETIVE FACILITIES

PARK	Monthly Patrons Served				YTD Patrons Served			
	(total program participants and non-program visitors)				(total program participants and non-program visitors)			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Lake St Clair	29,587	26,420	27,340	8%	64,481	67,611	61,827	4%
Wolcott Mill	4,448	3,789	4,780	-7%	16,423	13,969	15,706	5%
Wolcott Farm	25,567	5,496	15,795	62%	62,827	18,760	37,909	66%
Stony Creek	22,742	21,752	22,652	0%	68,828	82,592	77,148	-11%
Eastern Mobile Center	2,108	585	1,247	69%	12,145	8,317	7,908	54%
Indian Springs	11,821	13,377	12,691	-7%	28,791	31,727	30,846	-7%
Kens NC	32,025	28,985	31,684	1%	105,559	106,991	107,576	-2%
Kens Farm	29,734	35,876	35,226	-16%	99,953	105,489	108,547	-8%
Western Mobile Center	1,568	491	1,107	42%	5,082	4,980	6,214	-18%
Hudson Mills	3,620	2,771	3,325	9%	15,217	14,729	15,491	-2%
Oakwoods	15,032	14,401	15,117	-1%	50,373	59,388	58,211	-13%
Lake Erie	15,932	12,912	13,508	18%	58,779	56,098	50,975	15%
Southern Mobile Center	2,131	2,014	1,884	13%	7,054	4,014	4,435	59%
Totals	196,315	168,869	186,355	5%	595,512	574,665	582,795	2%

PARK	Monthly Revenue				YTD Revenue			
	Current	Previous	Prev 3 Yr Avg	Change from Average	Current	Previous	Prev 3 Yr Avg	Change from Average
Lake St Clair	\$ 5,899	\$ 6,862	\$ 6,502	-9%	\$ 10,875	\$ 15,248	\$ 14,010	-22%
Wolcott Mill	\$ 1,080	\$ 939	\$ 1,265	-15%	\$ 9,090	\$ 6,349	\$ 5,955	53%
Wolcott Farm	\$ 12,288	\$ 9,782	\$ 9,158	34%	\$ 40,320	\$ 39,988	\$ 36,264	11%
Wagon Rides	\$ 1,707	\$ 2,265	\$ 1,758	-3%	\$ 5,202	\$ 7,709	\$ 4,511	15%
Livestock/Produce	\$ 2,217	\$ 2,343	\$ 1,894	17%	\$ 10,679	\$ 13,052	\$ 11,678	-9%
Farm Total	\$ 16,212	\$ 14,390	\$ 12,809	27%	\$ 56,201	\$ 60,749	\$ 52,453	7%
Stony Creek	\$ 4,521	\$ 4,188	\$ 4,329	4%	\$ 12,364	\$ 13,048	\$ 12,199	1%
Eastern Mobile Center	\$ 2,038	\$ 430	\$ 237	761%	\$ 10,313	\$ 2,630	\$ 970	963%
Indian Springs	\$ 5,871	\$ 4,184	\$ 6,668	-12%	\$ 12,152	\$ 12,922	\$ 14,419	-16%
Kens NC	\$ 7,472	\$ 6,371	\$ 5,159	45%	\$ 12,777	\$ 13,600	\$ 12,684	1%
Kens Farm	\$ 20,790	\$ 4,743	\$ 5,295	293%	\$ 74,160	\$ 18,246	\$ 30,031	147%
Wagon Rides	\$ 1,790	\$ 3,725	\$ 2,976	-40%	\$ 8,115	\$ 16,720	\$ 13,673	-41%
Livestock/Produce	\$ -	\$ 150	\$ 412	-	\$ 1,604	\$ 3,420	\$ 8,048	-80%
Farm Total	\$ 22,580	\$ 8,618	\$ 8,684	160%	\$ 83,878	\$ 38,386	\$ 51,752	62%
Western Mobile Center	\$ 413	\$ 775	\$ 1,763	-77%	\$ 10,137	\$ 9,810	\$ 11,472	-12%
Hudson Mills	\$ 2,625	\$ 1,038	\$ 1,359	93%	\$ 7,866	\$ 5,705	\$ 7,139	10%
Oakwoods	\$ 3,740	\$ 3,258	\$ 2,985	25%	\$ 6,737	\$ 7,804	\$ 6,875	-2%
Lake Erie	\$ 1,963	\$ 1,676	\$ 1,469	34%	\$ 4,958	\$ 9,019	\$ 6,665	-26%
Southern Mobile Center	\$ 5,085	\$ 432	\$ 225	2157%	\$ 12,009	\$ 3,812	\$ 1,410	752%
Totals	\$ 79,498	\$ 53,160	\$ 53,453	49%	\$ 249,357	\$ 199,082	\$ 198,002	26%

BREAKDOWN OF ATTENDANCE	ON-SITE Programs and Attendance				OFF-SITE Programs and Attendance			
	CURRENT YEAR		PREVIOUS YEAR		CURRENT YEAR		PREVIOUS YEAR	
	Programs	Attendance	Programs	Attendance	Programs	Attendance	Programs	Attendance
Lake St Clair	108	5,777	187	4,684	-	-	-	-
Wolcott Mill	12	508	5	275	-	-	1	1,000
Wolcott Farm	99	2,923	83	2,105	-	-	1	40
Stony Creek	63	3,742	105	3,252	-	-	-	-
Eastern Mobile Center					48	2,108	22	585
Indian Springs	85	4,684	90	5,611	16	417	12	704
Kens NC	64	2,271	68	2,302	-	-	5	232
Kens Farm	108	2,725	138	3,113	-	-	-	-
Western Mobile Center					47	1,568	17	491
Hudson Mills	28	955	4	79	5	165	6	192
Oakwoods	74	1,565	30	731	2	83	-	-
Lake Erie	77	1,801	56	1,368	1	21	1	25
Southern Mobile Center					70	2,131	21	2,014
Totals	718	26,951	766	23,520	189	6,493	86	5,283
BREAKDOWN OF ATTENDANCE	OTHER VISITORS (Non-programs)							
	Current	Previous						
Lake St Clair	23,810	21,736						
Wolcott Mill	3,940	2,514						
Wolcott Farm	22,644	3,351						
Stony Creek	19,000	18,500						
Indian Springs	6,720	7,062						
Kens NC	29,754	26,451						
Kens Farm	27,009	32,763						
Hudson Mills	2,500	2,500						
Oakwoods	13,384	13,670						
Lake Erie	14,110	11,519						
Totals	162,871	140,066						

"ON-SITE" - Statistics includes both programs offered to the public and programs offered to school and scout groups.

"OFF-SITE" - Statistics includes outreach programs at schools, special events such as local fairs, or outdoor related trade shows.

"OTHER VISITORS" - Represents patrons to interpretive centers who visit to view exhibits, walk trails, and generally just enjoy the outdoors.